



CITY OF BLACK DIAMOND
November 13, 2008 Workstudy Agenda
25510 Lawson St., Black Diamond, Washington

6:00 P.M. – CALL TO ORDER, ROLL CALL

1.) Design Guidelines

Mr. Pilcher

ADJOURNMENT:

BLACK DIAMOND DESIGN
GUIDELINES

for

**Residential Uses in the
Historic Village Core**

The Historic Village Core Residential

Introduction and Purpose

These guidelines are intended to guide infill development within the Historic Village Core and Morganville that reflects the late 19th century vernacular characteristic of these areas. These guidelines are intended to apply to new construction and remodels and/or additions to existing homes that are over 75 years in age. Newer construction that may be located within these areas is not required to comply with these guidelines.

The guidelines are intended to encourage the preservation and restoration of historic homes within the city, recognizing that they are valuable community assets (both economically and aesthetically), to the town and its citizens.

The most common types of homes historically constructed in these areas are commonly described as “company cottages” or “vernacular” style. Homes were small in size, with standardized configuration. Although some homes are two story, most were built as one story with an attic and a large front porch. The material used was wood, usually over a stone foundation.

The areas impacted by these regulations are as shown on Figure 1, below.

BUILDING DESIGN

A. Building Entrances

Intent

To provide building facades that feature traditional building elements and details, such as gabled roofs, porches, dormers and cornice lines, that add visual interest and reduce apparent bulk and scale..

Guidelines

1. Include the following features in new residential building facades:
 - a. gable or hipped roof;
 - b. porches or entry enhancement, such as an overhang or a sidelight;
 - c. vertically proportioned windows;
 - d. eaves.
2. Design principal building entries to be visible from the street and directly accessible from the public sidewalk.
3. Accent principal entries by building elements (stairs, roofs, special fenestration, etc.) and elevation at least 18 inches above the grade level of the sidewalk or street.
4. Provide a recess, porch, portal or other protected exterior area that encourages human activity.
5. Retain existing historic doors or replace with doors that match historic doors in materials, size and style, series of panels and same dimension frames.

B. Windows

Intent

Provide traditional-oriented windows that highlight and accent a structure. Windows are vital elements of historic homes and are typically highlighted or accented. Historic window frames are wood, surrounded by a thick sash that accents and celebrates the window, while protecting it from wind and rain.

Guidelines

1. Reflect the fenestration patterns of the neighborhood in new construction.
2. Use vertically proportioned windows in new construction. Prohibit the use of horizontally proportioned windows. Grouping of vertically proportioned windows is acceptable.

3. For remodeling of existing structures, replace windows facing the street with new windows of similar style and proportions.

C. Rooflines

Intent

To add visual interest to a building and the street and complement neighboring structures by providing prominent roofs.

Guidelines

1. Roof designs shall incorporate at least one of the following:
 - a. gable or hipped roof;
 - b. broken or articulated roofline;
 - c. prominent cornice or fascia that emphasizes the top of the building;
 - d. other elements that emphasize a building's concept.
2. Create prominent pitched roofs with minimum slope of 6:12 vertical to horizontal ratio and maximum 12:12 ratio.
3. Allow different roof pitches for accessory structures not visible from the street.
4. For historic buildings, replicate the original shape or form of the roof in remodels or additions, unless the character of the original roof is not consistent with the type or style of roof of neighboring buildings.

D. Exterior materials and colors

Intent

Ensure that exterior materials relate to typical historic patterns and add visual interest, and are of durable, high quality and easy to maintain.

Guidelines

2.
 1. Use horizontal wood siding (4 to 6 inch shiplap, clapboard, or wood shingles). Use compatible materials (wood siding or brick) for detached garages.
3. Use muted colors for the background color of buildings, with brighter/lighter colors being appropriate for trim.

SITE DESIGN

A. Orientation to the Street

Intent

Historic homes are typically oriented to the street, site to make their entries and use clear to neighbors and visitors. These features provide for a pleasant streetscape that enhances pedestrian access and walking, promoting interaction among neighbors.

Guidelines

1. Provide a front façade facing the primary street, siting a structure to make its entry and residential use clear to approaching visitors.
2. Provide vehicular access from alleys where alleys exist.
3. Street-facing garages should be architecturally compatible with and never dominate a residence.
4. Locate driveways no closer than 3 feet to side property lines to provide room for landscaping.
5. For new buildings that project beyond the footprint of homes on adjacent lots:
 - a. limit the length and height of the projection into the rear yard area to reduce the impact of neighboring rear yards;
 - b. minimize windows, decks, and balconies overlooking neighboring yards or screen in order to protect privacy.

B. Fences and hedges

Intent

Prevent the installation of intrusive, nontraditional fences and tall hedges that cut the views of structures from the street.

Guidelines

1. Limit the height of front yard fences to no greater than 42 inches.
2. Use traditional materials such as wrought iron or wood pickets; prohibit the use of chain link fencing in front yards.



Historic Village Core Residential Design Standards and Guidelines

A. Purpose

- ◆ To encourage infill development within the Historic Village Core area and the Morganville area that reflects the late 19th Century Vernacular character of these areas.
- ◆ To encourage the preservation and restoration of historic homes within Black Diamond, recognizing that they are valuable assets, both economically and aesthetically, to the town and its citizens.
- ◆ To provide for a compact residential-oriented village core that contains a variety of uses within walking distance.
- ◆ To protect the property values by managing changes so they reinforce the assets of historic Black Diamond.
- ◆ To increase the awareness and appreciation for the historic heritage of Black Diamond.

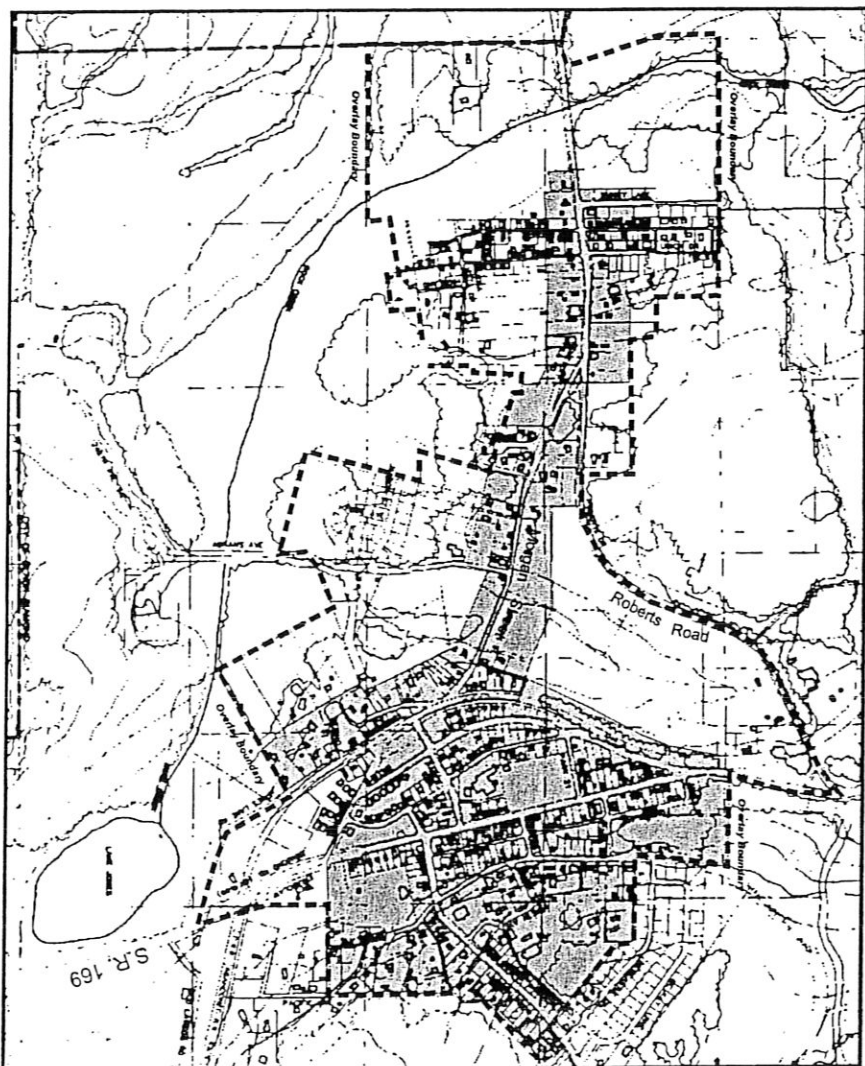


Figure HR-1. Historic Village Core - Residential Overlay (outlined with dashed line). Shaded areas signify properties that are most visible and may be subject to the Planning Commission or Review Board making "recommended" guidelines requirements on individual projects.

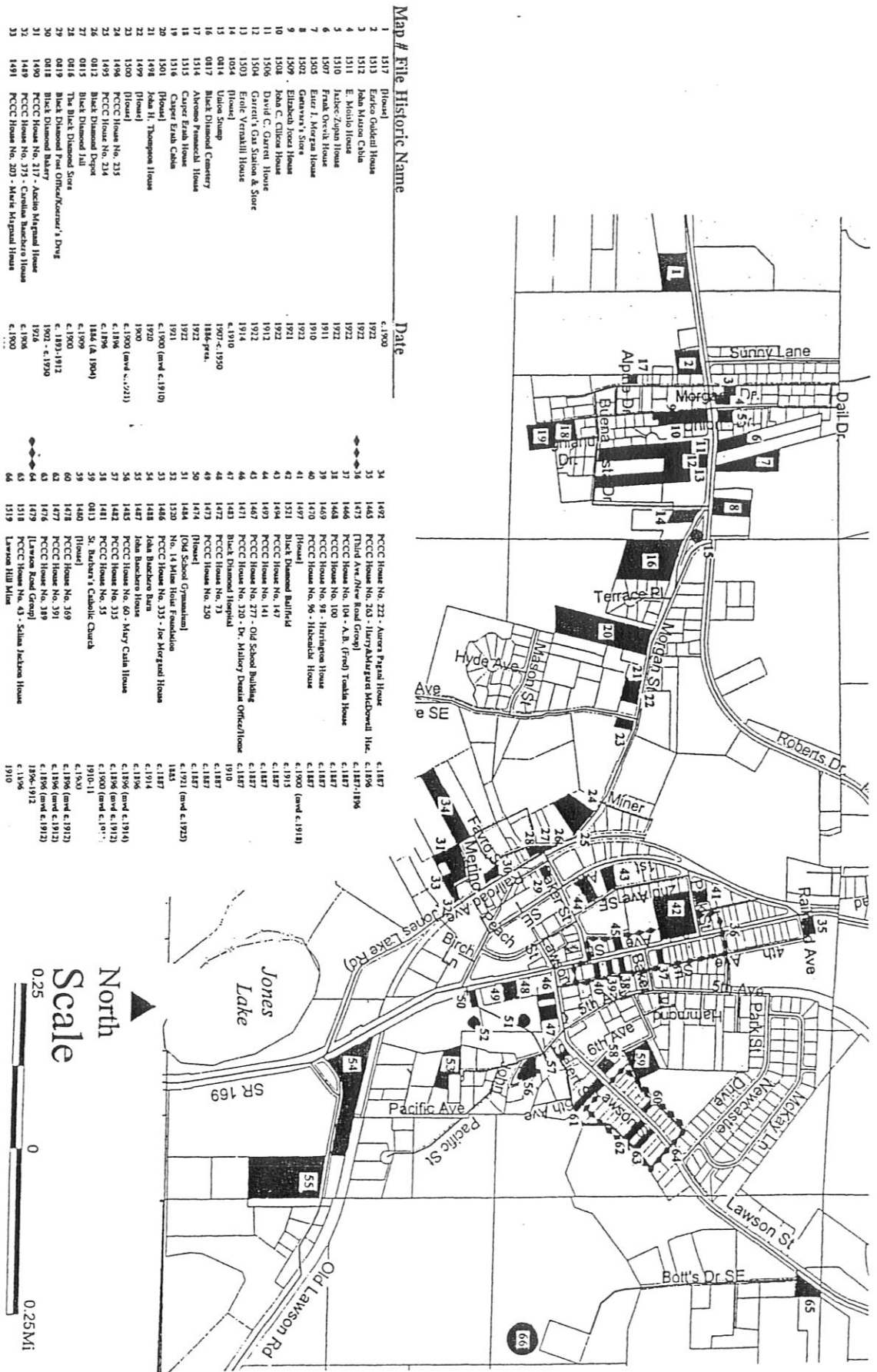


Figure HP-1. Black Diamond Historic Resource Inventory, 1998, King County Office of Cultural Resources.

BLACK DIAMOND DESIGN GUIDELINES

for

Multi-family Development

Introduction and Purpose

This section of the Design Standards and Guidelines focuses on site planning and design guidance for new multi-family development. The city's historic development patterns reflect several eras of growth and annexation. The residential neighborhoods of the historic town-site and Morganville are composed of small lots in traditional grid patterns and narrow streets. Other areas have developed in a non-grid pattern with larger lots. Now, with the rate of new development likely to increase significantly over the next 20 years, the city seeks to accommodate residential development with a mix of types, sizes and densities, clustered to retain the open space and rural land uses that form the area's natural beauty, and to connect housing, shopping, employment and recreation in a more efficient manner. With a maximum density of 12 units per acre, how new housing types such as townhouses, cottage housing and courtyard housing are designed will be crucial in ensuring a proper "fit" with the surrounding community. The design guidelines are organized in the three categories below.

Site Design

Site planning arranges building masses, open space, parking and circulation to create a site design that is orderly, visually pleasing, and that contributes positively to both the surrounding area and the development itself. Historic development patterns in Black Diamond relate to the street, encouraging people to participate more fully in their community. These site design guidelines will promote neighborhood compatibility, retention of natural features, integration with the surrounding community, opportunity for social interaction, and a safe, comfortable, and interesting environment for residents.

Building Design

The building design guidelines address the overall external appearance of multi-family development, including building forms, details, and proportions. Use of single-family residential design elements are recommended to reduce perceived density, give character to the development and its individual dwelling units, add visual interest, and be compatible with the neighborhood context.

It is not intended that these guidelines prescribe one architectural style or a specific design character. There are various architectural styles found in Black Diamond's housing stock that help create unique settings. The primary focus should be to construct a high quality residential environment within the context of the existing community. Quality development is further encouraged through a sustainable design approach.

Landscaping

The rolling topography and variety of open pastures and meadows, wetlands and forested areas in the city help define its character. New developments can establish visual connections with these natural features through the choice and placement of landscape features. Moreover, an attractive, well-maintained landscaped environment contributes to residents' quality of life and also enhances the appearance of the surrounding neighborhood.

SITE DESIGN

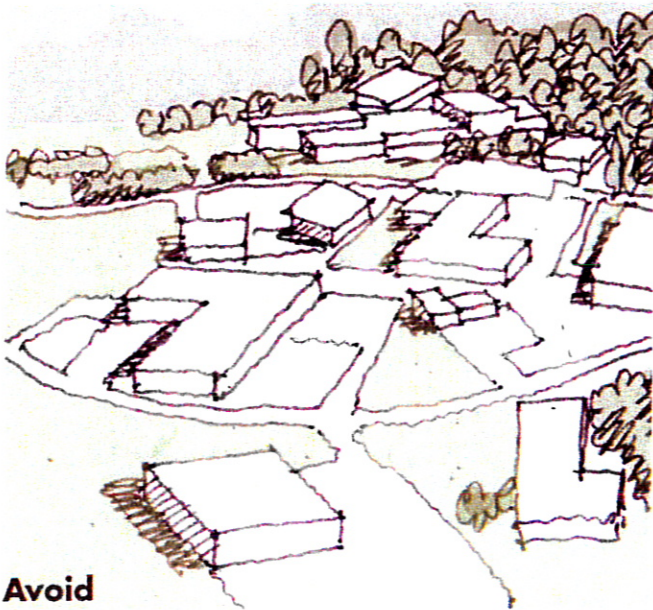
A. Context

Intent

Multi-family development successfully contributes to the overall community when relationships with the existing and planned land uses, development patterns, and context are considered.

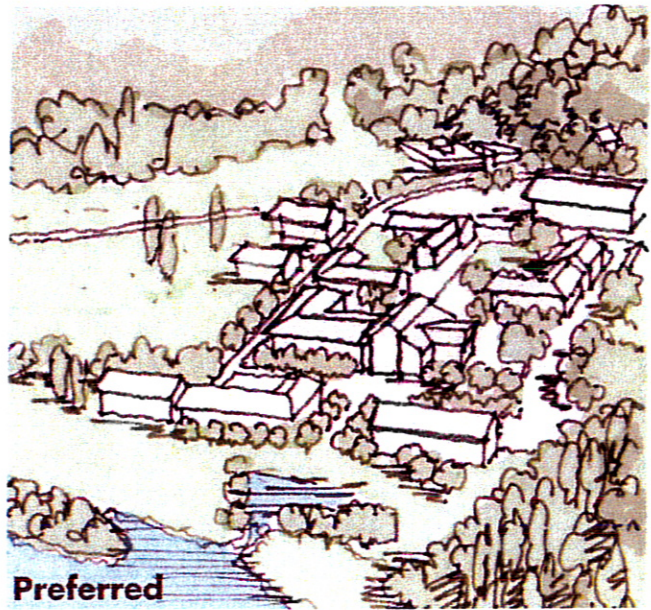
Guidelines

1. Incorporate existing unique site amenities such as views, mature trees, and similar natural features into developments whenever possible.
2. Use complementary building arrangements, buffers, and avoidance of overwhelming building scale to endure that new multi-family residential development is compatible with residential development in the immediate area. For example, cottage housing is encouraged in older, smaller lot residential neighborhoods to complement similar siting in the area.
3. Complement existing landscape materials, location, and massing on adjacent developments with new landscape plantings.



Avoid

Orient and cluster buildings, and design parking, landscaping and open space in ways that complement view sheds and surrounding natural features.



Preferred

SITE DESIGN

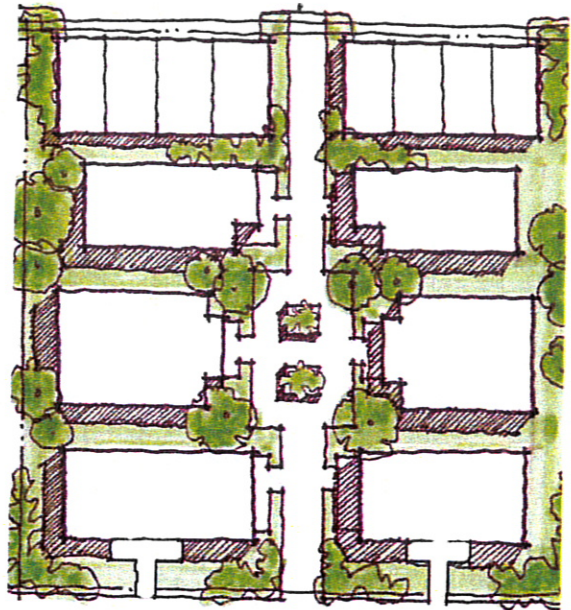
B. Building Siting

Intent

Appropriate building siting can reduce the perceived density of multi-family developments, maximize open space areas, provide “eyes on the street” surveillance, and enhance neighborliness by creating community gathering spaces.

Guidelines

1. Orient developments parallel to the public street, with setbacks from the street used to create landscaped open space.
2. To create an appropriate transition from residences and the street, use the space between the building and sidewalk to provide security and privacy for residents and to encourage social interaction among residents and neighbors.
3. Doors shall be visible from the street and windows should allow residents to have “eyes on the street” for surveillance.
4. Cluster multi-family buildings around courtyards, gathering areas and open spaces.
5. Repeat these design elements throughout portions of the development that are not oriented to the street.

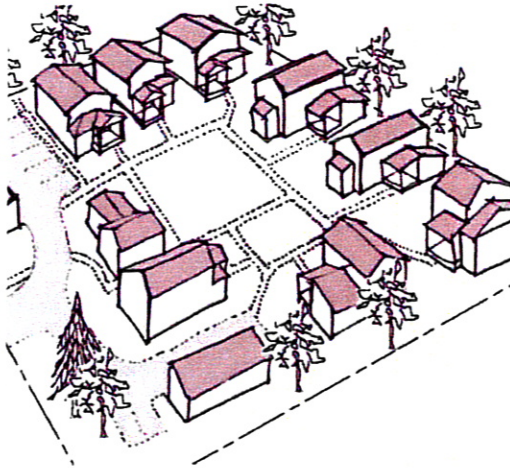


street

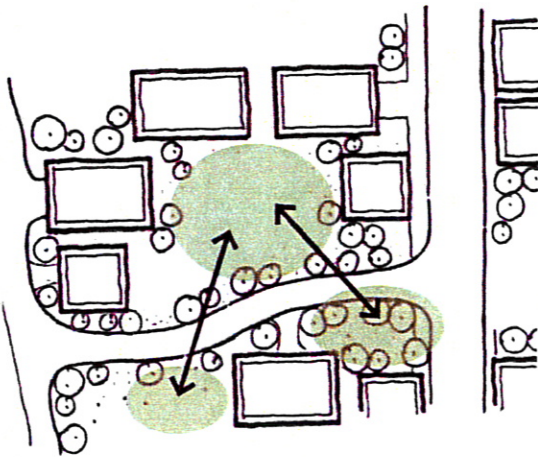
Buildings sited parallel to the public street and also configured to form centralized open space area



Locate shared amenities such as outdoor play areas and bicycle parking



Centralized open space provides convenient access for many units



Large and small open spaces are connected and oriented to dwellings



Private open space at rear of unit

C. Open Space

Intent

Common open space provides opportunities for casual social interaction and safe play areas for children, and it reduces the perceived density of the development. Private open space serves as an outdoor room for residents.

Guidelines

1. Conveniently locate open space to the majority of units for recreation and social activities.
2. Shelter open spaces should be sheltered from the noise and traffic of adjacent streets or other incompatible uses. Take advantage of sun orientation to provide a comfortable environment.
3. Provide well-defined open space edges through the use of walkways, buildings or landscaping.
4. For larger developments, provide a series of connected open space areas of varying shape, appearance and intended use.
5. Require private open space (such as yard, patio or balcony) that is visible and can be entered from inside the dwelling for all units.
6. Define boundaries between private and common open spaces by elements such as low walls or plant materials.
7. Site and design buildings so that windows of neighboring units do not overlook private open spaces.
8. Provide outdoor play areas for children. Play areas shall not be located near public streets, parking or entry areas unless physically separated by appropriate barriers such as walls or fencing.

C. Open Space *CONTINUED*

Intent

Common open space provides opportunities for casual social interaction and safe play areas for children, and it reduces the perceived density of the development. Private open space serves as an outdoor room for residents.

Standards

| Housing Type | Private | Common |
|--------------|---|---|
| Cottage | Minimum 200 sf useable open space per unit separated from common open space by hedge or fence not to exceed 36" in height | Minimum 400 sf per cottage Minimum 20 ft. width At least 50% of cottage units abut common space |
| Courtyard | Minimum 200 sf useable open space per unit. | Minimum 300 sf per unit for courtyard space. Minimum 25 ft width At least 50% of units abut courtyard space |
| Townhouse | Minimum 300 sf useable open space per unit at ground level | Minimum 75 sf per unit provided for development of 4 or more units |

D. Parking/Vehicular Circulation

Intent

Safe and efficient circulation and parking arrangements take into consideration the needs of pedestrians, children at play, parking lot appearance, and safety.

Guidelines

1. Locate parking areas in the development's interior and not along street frontages. Minimize driveway openings along street frontages.
2. Avoid blank walls with rows of garage doors that face the public street front. Use of single-car garage doors, rather than double-car garage doors. Provide landscaping around garages, and tuck-under parking.
3. Avoid a single large parking area where cars would dominate views and increase the perceived density of the site. Break up parking areas at least every six stalls by landscaping, change in paving materials and walkways into smaller parking "courts" with convenient access that relates to adjacent dwelling units. Provide sight lines from dwelling units out to parking areas. For security purposes, filter through use of landscaping, including trees.
4. Include an adjacent pedestrian entry path that connects to the public sidewalk along all entry drives. Provide elements that define the main entry, including lighting, textured paving, and accent plant materials such as specimen trees and flowering plants.
5. Control vehicle speeds by appropriate signage, changes in roadway texture, and other traffic calming devices, if necessary.
6. Clearly identify visitor parking in the development.

E. Pedestrian Circulation

Intent

Pedestrian circulation provides safe, efficient access to facilities and dwelling units for residents, encourages casual social interaction, and allows natural surveillance by residents.

Guidelines

1. Where appropriate, incorporate safe pedestrian connections to public sidewalks and to adjoining residential developments, commercial projects, and other compatible land uses.
2. Provide pedestrian access to adjacent existing or planned open space areas and trails.
3. Avoid combining vehicle and pedestrian access to dwelling units as this does not allow adequate room for landscaping and other features that can personalize the front entry.
4. Locate walkways should to minimize the impact of pedestrians on the privacy of nearby residences or private open space. Provide a landscaped planting area between walkways and building facades.
5. Walkways shall be accessible to disabled persons and in conformance with the Americans with Disability Act (ADA).

SITE DESIGN

H. Access to Dwellings

Intent

Access to dwellings can provide a unique opportunity for individuals to personalize their space, allow opportunities for social interaction, and increase natural surveillance with additional features such as porches.

Guidelines

1. Ensure the main entry to each dwelling unit is clearly visible from the nearest public circulation walkway. Provide a porch, covered stoop, or similar entry feature at each unit's front entry.
2. Each individual unit shall have its own private walkway to the front door at ground level.



*Walkways combined with enhance entry feature lead to each unit.
Doors and windows encourage "eyes on the street".*

BUILDING DESIGN

A. Building Mass

Intent

Reduce the apparent bulk of multi-family buildings and maintain a pedestrian scale compatible with Black Diamond's small town character.

Standards

| Cottage Housing | | |
|--|--|------------------------|
| Development Standards | By Right | Flexibility Available? |
| Minimum Lot Size | 2800 sf | no |
| Maximum Lot Size | none | no |
| Floor Area Ratio | none | |
| Maximum Lot Coverage (all impervious surfaces) | 70% | no |
| Front Yard Setback | Average of 10 ft, throughout the project; not less than 5 ft. | Yes |
| Side Yard Setback | 5 ft. | no |
| Rear Yard Setback | 10 ft. | no |
| Maximum First Floor Area | 50% of units, can not exceed 650 sf; 50% can not exceed 800 sf | yes |

| Townhouse | | |
|--|--|------------------------|
| Development Standards | By Right | Flexibility Available? |
| Minimum Lot Size | 7200 sf | no |
| Maximum Lot Size | based on maximum number of units allowed (12?) | no |
| Floor Area Ratio | | |
| Maximum Lot Coverage (all impervious surfaces) | 60% | no |
| Front Yard Setback | min. 10 ft/max. 25 ft | Yes |
| Side Yard Setback | 5 ft. | no |
| Rear Yard Setback | 5 ft. with an alley/ 10 ft without | no |
| Maximum number of attached units per structure | 6 | yes |

BUILDING DESIGN

A. Building Mass

Intent

Reduce the apparent bulk of multi-family buildings and maintain a pedestrian scale compatible with Black Diamond's small town character.

| Courtyard | | |
|--|--|------------------------|
| Development Standards | By Right | Flexibility Available? |
| Minimum Lot Size | based on minimum number of units allowed (4?) | no |
| Maximum Lot Size | based on maximum number of units allowed (12?) | no |
| Floor Area Ratio | | |
| Maximum Lot Coverage (all impervious surfaces) | 60% | no |
| Front Yard Setback | min. 10 ft/max. 25 ft | Yes |
| Side Yard Setback | 5 ft. | no |
| Rear Yard Setback | 5 ft. with an alley/ 10 ft without | no |
| Maximum number of attached units per structure | 6 | yes |

BUILDING DESIGN

B. Building Scale and Character

Intent

To help retain the city's small town character through building design.

Guidelines

1. Incorporate smaller-scale forms such as bays, recessed or projecting balconies, and dormers into the design to visually reduce the height and scale of the building and to emphasize the definition of individual units. Include architectural elements such as bay windows, porches, projecting eaves, etc. that add visual interest to the development.
2. Where a neighborhood has a recognizable architectural context, such as Morganville, use those building features as visual cues for incorporation into the development's design.

Roofs

1. Use residential roof pitches and materials that reflect the prevailing roof types in the neighborhood, such as hipped or gabled roofs.
2. Vary roof lines within the overall horizontal plane through combinations of roof heights that create variation and visual interest.
3. Incorporate the roof pitch and materials of adjacent buildings into carport or garage roofs.



Roof pitches, dormers and change in upper level materials break down the scale of a multi-family development and relate to single family character nearby

C. Facade Modulation

Intent

To avoid boxy and monotonous facades that lack human scale dimensions and have large expanses of flat wall planes.

Guidelines

1. Provide a jog in the building facade (modulation) between every unit. Modulation elements shall have a minimum 2-foot projection or recession from the facade, and be a minimum of 6 feet in length.
2. Use architectural treatments, such as recessed windows, moldings, decorative trim, and wood frames to add three-dimensional quality and shadow lines to the facade.
3. Use windows of varied shape, size, and placement are encouraged.
4. Incorporate architectural detailing consistent with the development's overall design into garage doors, such as patterned garage doors, painted trim, or varied colors.



Bays, dormers, balconies and other projected or recessed design elements reduce the building's mass and add visual interest

BUILDING DESIGN

E. Building Entries

Intent

To create a socially and visually stimulating commercial district with street level facades that support pedestrian activity.

Guidelines

1. Attractively design courtyard entry gates as an important architectural feature of the building or development.
2. Emphasize and differentiate each individual unit's entry through architectural elements such as porches, stoops, or roof canopies, and detailing such as paint color, trim, materials or awnings. Provide opportunities for residents to personalize individual entries by providing ground level space or a wide ledge for plants and other features.



Low walls and landscaped areas help define the transition from public to private space. Ledges create opportunities to personalize the dwelling unit



Well designed entry gate

Landscaping

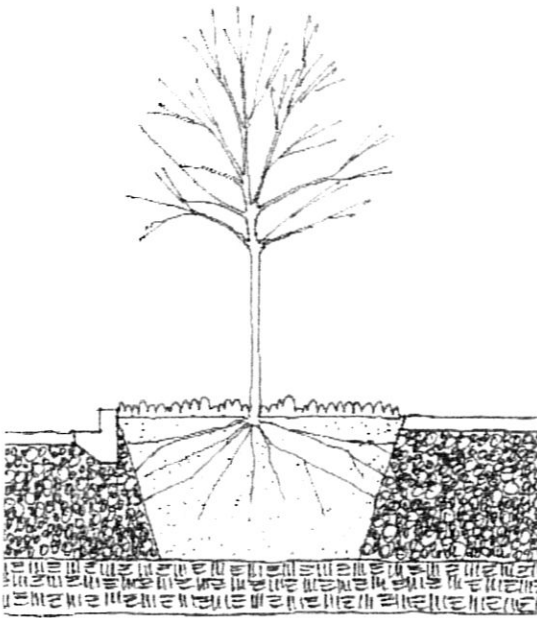
A. Landscape Design

Intent

To create unique identity, establish visual connections to surrounding natural areas, soften the architecture, provide shade and screen unattractive areas.

Guidelines

1. Use a three-tiered landscape planting approach consisting of ground cover; shrubs and vines; and trees.
2. Use different landscape design and plant materials in the various courtyards and common open space areas of the development to create an individual identity for each space.
3. Use landscape plantings to help define property lines and distinguish private space from public space through a change in plant material, form or height.
4. Select trees and shrubs based on their mature size and root characteristics. Plants with root systems that uplift hardscape materials or are considered invasive shall be avoided.
5. Sixty percent of plantings materials shall be a native species.



Generous planting areas and uncompacted soil help sustain healthy landscaping

Landscaping

BLACK DIAMOND DESIGN GUIDELINES

for

The Town Center

The Town Center

Introduction and Purpose

The 1996 Comprehensive Plan established a blueprint for growth that will both preserve and enhance the city's physical characteristics and community values. These design guidelines for the Town Center District provide specific recommendations to ensure that this district can maintain its unique, semi-rural, small town atmosphere while accommodating compact forms of new development.

Site Design

New development in the Town Center district should place a strong visual emphasis on the street, and support active public space. Fulfillment of these broad design objectives requires an integrated approach to site planning. Buildings, parking, service access, plazas and courtyards must each recognize the important relationship between public and private space.

Building Design

The overriding objective of the building design objectives is to establish a high standard of design for new development within the community. The hallmark of design excellence will be to create places of enduring quality that uniquely fit into the district's small town setting. In particular, new development adjacent to historic structures should respect the existing fabric and provide a transition between the old and the new.

SIDE DESIGN



Seating

A. Orientation to the Street

Intent

The life of the Town Center district is closely tied to the character of its public space. Collectively, buildings configure and shape the streetscape and other open spaces. New buildings should be located towards the street, at or near the sidewalk to promote community commercial activity, and heighten the presence and maintain the identity of the historic town center.

Guidelines

1. Locate and orient buildings to define public streets and civic spaces. In general, build up to the sidewalk and limit gaps to the minimum necessary to accommodate parking and access. Buildings with no setback need to include two or more of the following. Buildings that setback up to 10 feet from the back of sidewalk need to include three or more of the following:
 - a. Covered porch or boardwalk
 - b. Pedestrian scale bollard or other accent lighting
 - c. Special paving, such as colored/stained concrete, brick, or other unit paver
 - d. Seating, such as benches, tables, or low seating walls
 - e. Water feature
 - f. Sculptural art



Covered porch



Bollards

4

SITE DESIGN

B. Parking Lot Location

Intent

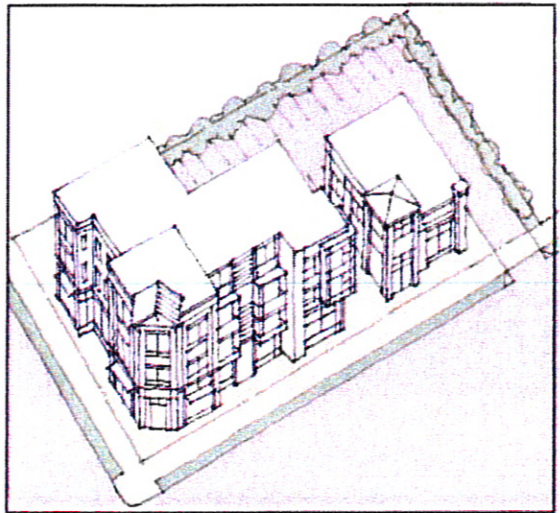
To locate parking in a manner that is as visually unobtrusive as possible, and provides safe, convenient access without detracting from the pedestrian environment.

Guidelines

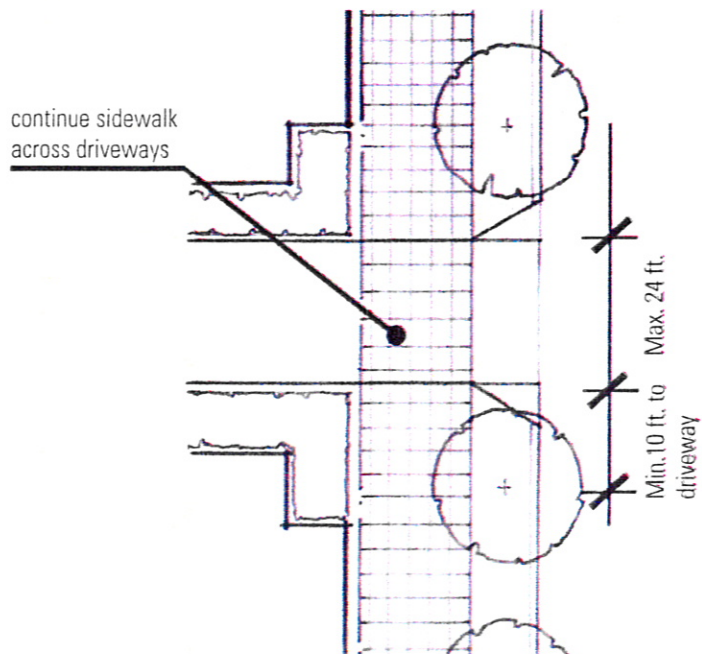
1. Locate parking lots to the side or behind buildings when possible.
2. Parking lots shall not abut street intersections (corner lots).
3. Enhance pedestrian safety by reducing and consolidating driveways, while providing for adequate access:
 - a. Distance between curb cuts in the same parcel or development should not be less than 100 feet.
 - b. Continue sidewalk pattern and material across the driveway.
 - c. Driveways shall not exceed 24' in width.
4. No parking stalls or drive lanes shall be placed between the building and the street.



Parking to the side of the building



Parking to the rear of the building



SITE DESIGN



Screen surface parking from public view

C. Parking Lot Screening

Intent

To reduce the impacts of surface parking.

Guidelines

1. Screen parking lots located beside buildings from the right-of-way with one or more of the following treatments:

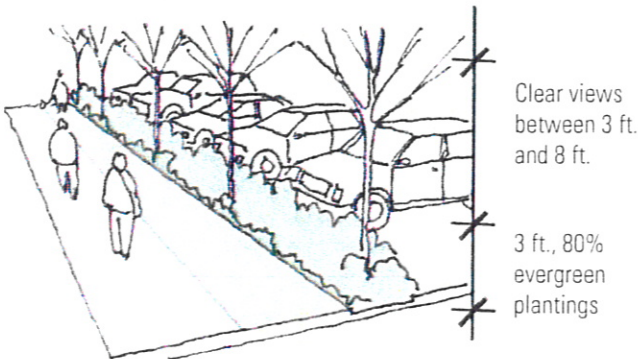
- a. Landscaping to sufficiently screen the first 3 feet in height.
- b. Low walls made of concrete, masonry, or other similar material and not exceeding a height of 3 feet.
- c. Raised planter walls planted with a minimum of 80% evergreen shrubs not to exceed 3 feet in height.

2. Reduce visual impact of surface parking lots.

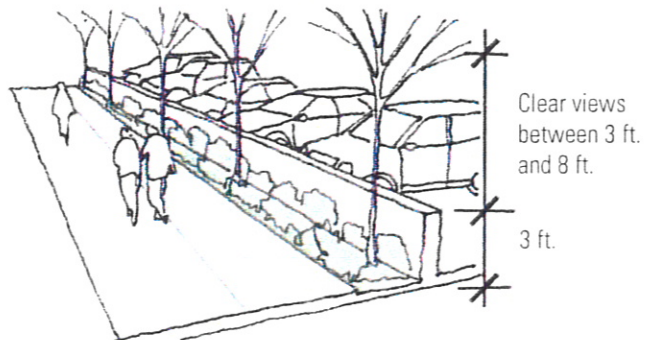
-Where the parking lot is located to the side of the building and partially abuts the public right-of-way, one tree for every six spaces shall be provided;

-Where the parking is behind the building and not visible from the public right-of-way, one tree for every eight spaces shall be provided.

note: no planting strip shall be less than 3 feet wide



Example landscaping meeting screening requirement



Examples of low wall and landscaping

SITE DESIGN

D. Pedestrian Connections

Intent

The commercial district should connect through a network of public spaces and paths. New development should improve the pedestrian environment by making it easier, safer, and more comfortable to walk between businesses, to the sidewalk, to transit stops, and through parking lots.

Guidelines

1. Clearly define pedestrian connections with a combination of two or more of the following ways:
 - a 6 inch vertical curb in combination with a raised walkway.
 - a trellis, special railing, bollards, and/or other architectural features to accent the walkway at key points.
 - a continuous landscape area minimum 3 feet wide on at least one side of the walkway.
2. Pedestrian connections should be reinforced with pedestrian-scale lighting to aid in way-finding.
3. Pedestrian walkways should include clear sight-lines to building entrances and should not be less than 4 feet wide.



Provide separated pedestrian connections to and through parking



Walkways should connect to the surrounding city pedestrian network, including sidewalks and crosswalks

SITE DESIGN



E. Screening of Trash and Service Areas

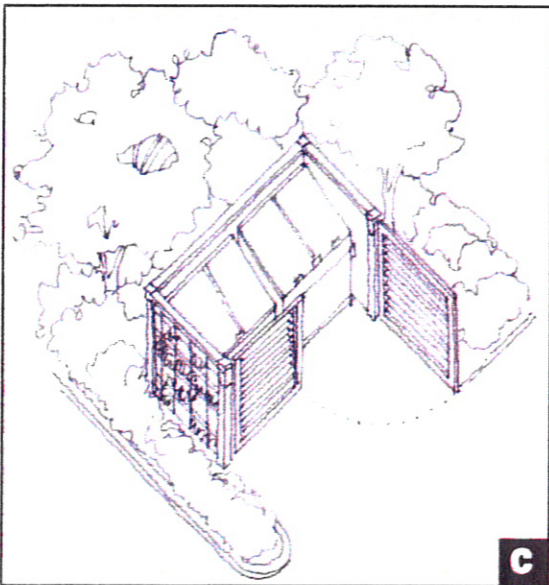
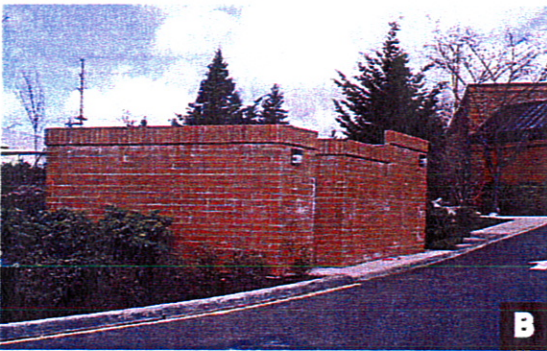
Intent

To reduce the impact of service, loading, and trash/recycling areas.

Guidelines

1. Screen service, loading and trash/recycling collection areas from public view with solid evergreen plant material or architectural treatment similar to the design of the adjacent building.

note: alternatives may be considered if designs meet the intent and are approved by the Community Development Director



screening with natural material (a), brick and plantings (b), and a combination of heavy wood and vine covered trellis (c)

BUILDING DESIGN

A. Building Entrance

Intent

To ensure that entrances are easily identifiable and accessible from streets and sidewalks.

Guidelines

1. Locate primary entrances so that they are visible from the public right-of-way.
2. Main entrances shall have weather protection in the form of an awning, canopy or covered porch.
3. Main entrances shall include two or more of the following:

Pedestrian lighting

Decorative paving

Special materials like ornamental tile

Side lights

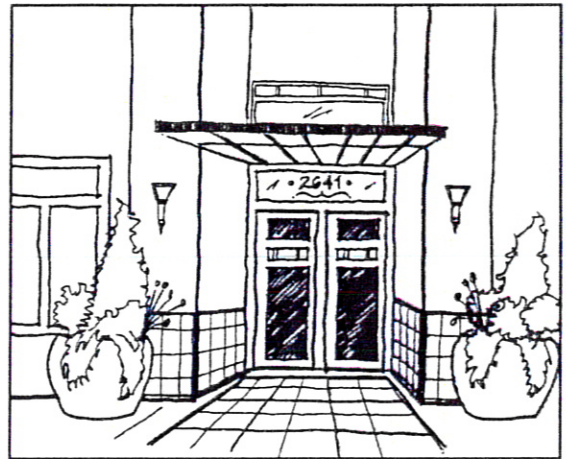
Clerestory

Landscaped pots and/or window boxes

Corner Entrance



Consider placing the main building entrance at a street corner, where feasible



Use design elements such as light fixtures, potted plants, canopies and special paving to accentuate a buildings entrance at the street

BUILDING DESIGN



Vary the three dimensional character of the building from bottom to top

B. Massing and Bulk

Intent

Reduce the apparent bulk of multi-story buildings and maintain a pedestrian scale compatible with Black Diamond's character.

Guidelines

Buildings above 30 feet in height shall:

1. Distinguish a “base” at ground level using articulation and materials such as stone, masonry, or decorative concrete.
2. The “top” of the building will emphasize a distinct profile or outline with elements such as a projecting parapet, cornice, upper level setback or pitched roof line.
3. The “middle” of the building may be distinguished by a change in materials or color, windows, balconies, or stepbacks.



Rely on building massing and materials to place a strong visual emphasis on the street; upper level is both set back and broken up into separate volumes to reduce the perceived scale of the building

BUILDING DESIGN

C. Transparency

Intent

Improve the pedestrian experience and safety by providing vision glass at the ground level.

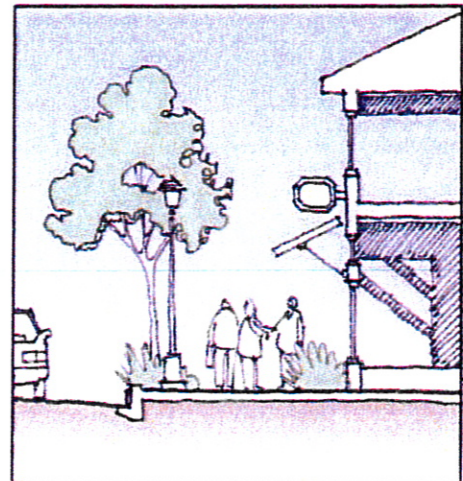
Guidelines

For new commercial buildings in the Town Center:

1. A minimum of 60% of any ground floor facade between 2 feet and 12 feet above grade and facing a street or public space shall be comprised of clear, "vision" glass.



Generous street-level shop windows and entrances animate the street



transparency between 2' and 12'

BUILDING DESIGN



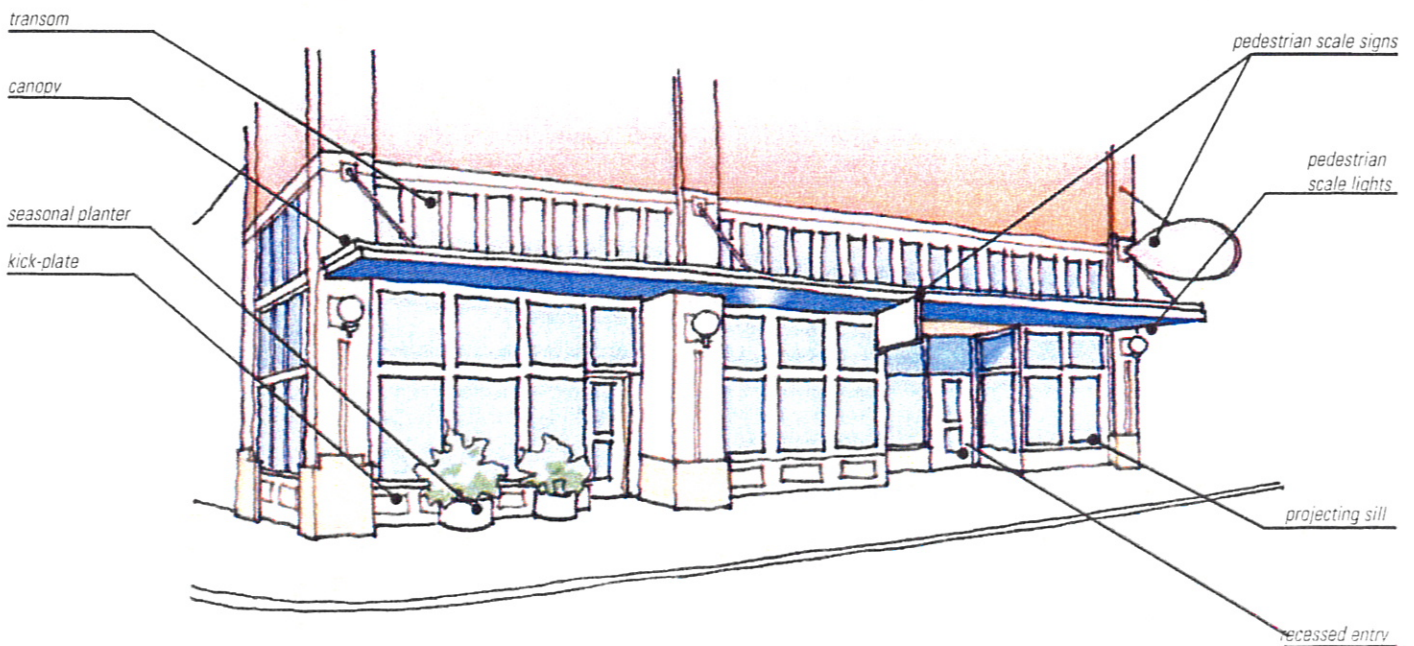
D. Ground Level Details

Intent

To create a socially and visually stimulating commercial district with street level facades that support pedestrian activity.

Guidelines

1. Reinforce the character of the streetscape with the greatest amount of visual interest placed along the ground level of buildings. Facades of commercial and mixed-use buildings shall be designed to be pedestrian-friendly through the inclusion of at least three of the following elements:
 - a. kickplates for storefront windows
 - b. projecting window sills
 - c. pedestrian scale signs
 - d. canopies
 - e. plinth
 - f. ornamental tilework
 - g. lighting or hanging baskets supported by ornamental brackets
 - h. an element not listed here that meets the intent



BUILDING DESIGN

E. Blank Wall Treatments

Intent

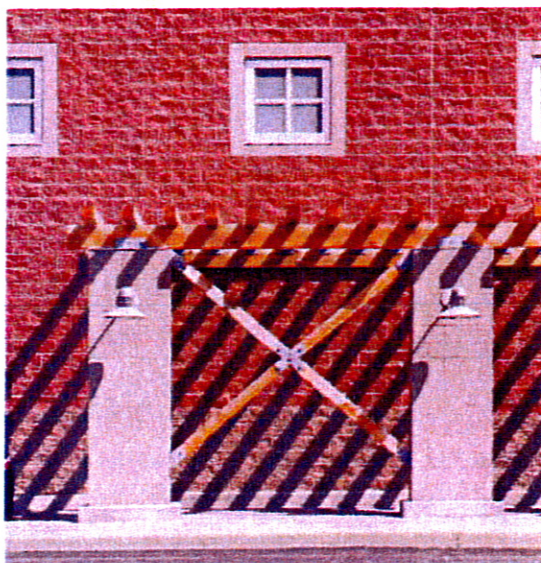
To reduce the visual impact of blank walls by providing visual interest.

Guidelines

1. Blank walls longer than 30 feet that face streets or are visible from residential areas shall incorporate two or more of the following:
 - a. Vegetation, such as trees, shrubs, ground cover and/or vines adjacent to the wall surface;
 - b. Artwork, such as bas-relief sculpture, murals, or trellis;
 - c. Seating area with special paving and seasonal plantings;
 - d. Architectural detailing, reveals, contrasting materials or other special visual interest.



Ground cover and vines help soften a flat facade



Architectural elements including building base, columns, lighting, trellis and faux windows



Artwork can provide visual interest and reveal a place's culture and history

BUILDING DESIGN



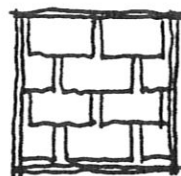
F. Historical Context

Intent

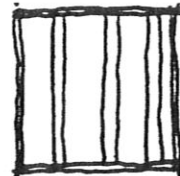
To promote building design that is sensitive to the overall character of Black Diamond.

Guidelines

1. Incorporate architectural elements into new development that reinforce the established character of Black Diamond including:
 - natural materials
 - window proportions
 - facade and canopy lines
 - covered boardwalks
 - front porch or stoop
 - extended parapets
 - decorative railings



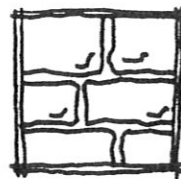
wood shingles



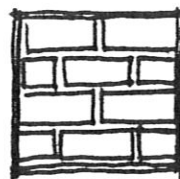
board & batten



ship-lap



stone



brick



exposed log

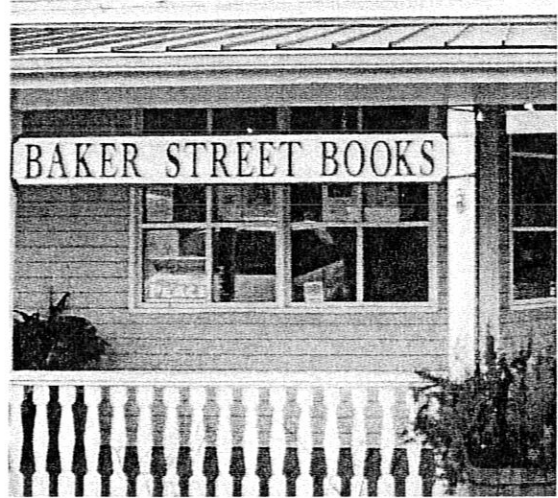
SIGN DESIGN

A. Sign Design*Intent*

To encourage small-scale, pedestrian-oriented signage that is compatible with the rural, historic character of Black Diamond.

Guidelines

1. Required sign placement:
 - a. On or under a marquee's, awning's, or canopy's vertical face (valance);
 - b. On the building facade, integrated with the architectural elements of the building;
 - c. On lower story windows.
2. Internally lit or backlit signs and billboard wall signs are prohibited.
3. Allow increased size for signs that are highly graphic in form, expressive and individualized.



Town Center Precedent: small-scale signs add individual expression of the storefront and detail and richness to the street level facade

BLACK DIAMOND

DESIGN GUIDELINES

for

Commercial Zones



Commercial Zones

Introduction and Purpose

This section of the Design Standards and Guidelines focuses on site planning and design guidance for the Commercial Zones of the city. These standards and guidelines serve to promote successful and vibrant commercial activity in the city while maintaining the rural and historic character that defines Black Diamond.

Site Design

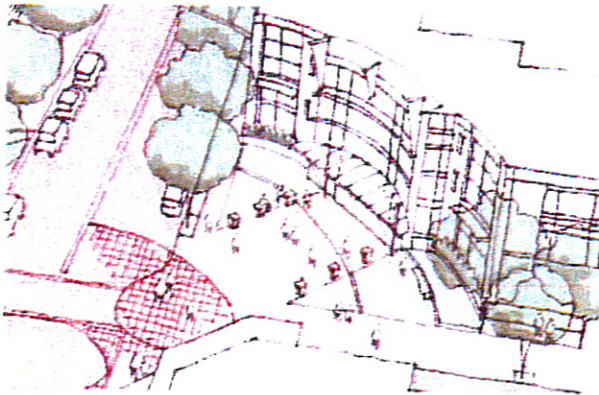
Site design in Commercial Zones serves to orient buildings and transportation corridors in a way that allows for pedestrian and vehicular access while screening undesirable features such as trash areas and parking lots. A focus on landscaping and connectivity to adjacent uses will be stressed to meet the overall site design objectives.

Building Design

Building design in Commercial Zones will be compatible with the historic and rural context of the City of Black Diamond. These standards and guidelines ensure the objectives are met by addressing building design features individually to maintain pedestrian friendly and visually appealing commercial areas that cater to residents and visitors alike.



Storefronts and entrances should enhance the activity on the street



Pulling back the entrance from the corner and providing additional sidewalk and landscaping

A. Orientation to the Street

Intent

The life of the commercial district is closely tied to the character of its public space. Collectively, buildings configure and shape the streetscape and other open spaces. New buildings should be located towards the street, near the sidewalk to promote community commercial activity, and to heighten the presence and improve the identity of commercial areas.

Guidelines

1. Locate and orient buildings to define public streets and civic spaces. Locate buildings close to the sidewalk and limit gaps to the minimum necessary to accommodate parking and access. Allow larger developments to pull back from the street edge for plazas and entry forecourts. Plazas, courtyards and other pedestrian space should include at least three of the following:
 - a. Special landscape
 - b. Pedestrian scale bollards
 - c. Accent lighting
 - d. Special paving, such as colored/stained concrete, brick or other unit paver
 - e. Seating, such as benches, tables, or low seating walls
 - f. Water feature
2. Provide clear, recessed main building or shop entrances that do not interrupt street and/or retail continuity.
3. Set back buildings at the corners of intersections from the corner property lines at the ground floor to allow for both a more generous sidewalk and additional street landscaping to support a high level of activity and visibility.

SITE DESIGN

B. Parking Lots

Intent

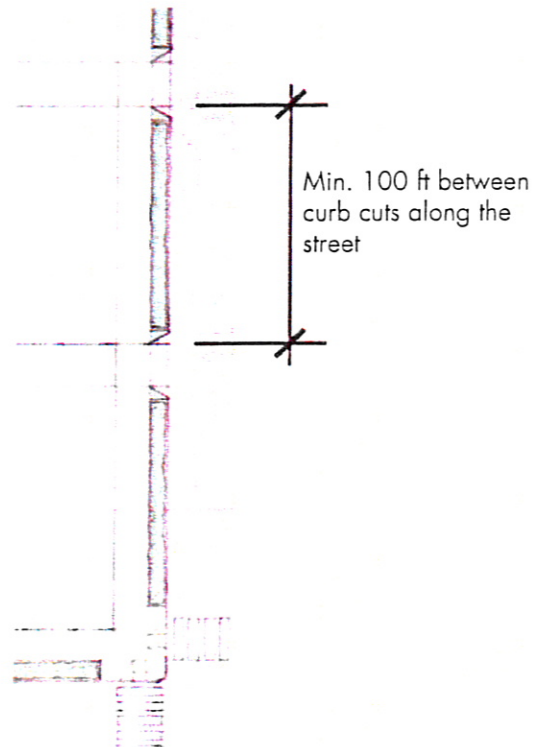
To locate parking in a manner that is as visually unobtrusive as possible and provides safe, convenient access without detracting from the pedestrian environment.

Guidelines

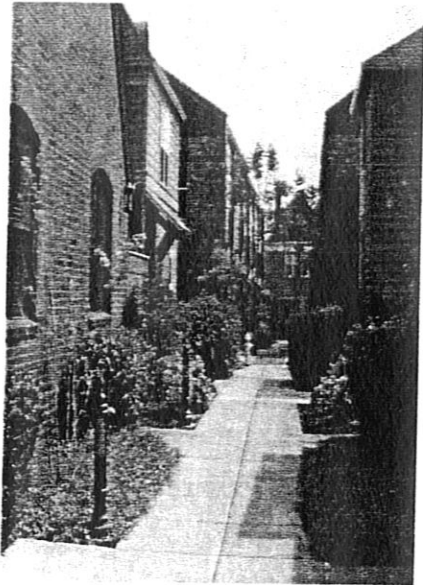
1. Locate parking lots behind buildings when possible.
2. Parking lots shall not abut street intersections (corner lots).
3. Where parking lots are beside buildings, screen them from the right-of-way with one or more of the following treatments:
 - a. Low walls made of concrete, masonry, or other similar material and not exceeding a height of 3 feet.
 - b. Raised planter walls planted with a minimum 80% evergreen shrubs with a total height not to exceed 3 feet.
4. Enhance pedestrian safety by reducing and consolidating driveways, while providing for adequate access:
 - a. Distance between curb cuts should not be less than 100 feet.
 - b. The sidewalk pattern and material shall continue across the driveway.
5. Require adjacent developments to share driveways.
6. Reduce the visual impact of surface parking lots.
 - a. Where the parking lot is located to the side of the building and partially abuts the public right-of-way, one tree for every six spaces shall be provided;
 - b. Where the parking is behind the building and not visible from the public right-of-way, one tree for every eight spaces shall be provided.



Parking lot screened with low walls and landscaping



Pedestrian connections can be maintained in larger parking lots



Connections provide safety and comfort through large sites

C. Pedestrian Connections

Intent

The commercial district should connect through a network of public spaces and paths. New development should improve the pedestrian environment by making it easier, safer, and more comfortable to walk between businesses, to the sidewalk, to transit stops, and through parking lots.

Guidelines

1. Pathway Widths and Surfaces:

| | WIDTHS | SURFACE MATERIAL |
|------------------------------|----------------|---|
| Business Entry Walkways | 8 feet or more | Concrete and/or pavers |
| Parking Lot Pathways | 5-8 feet | Concrete and/or pavers |
| Business Connecting Walkways | 8-16 feet | Concrete, pavers, gravel, stone or brick |
| Multi-purpose Regional Trail | 5-12 feet | Concrete, asphalt, gravel, stone, brick or wood |

2. Provide adequate building lighting at entries and along all walkways and paths through parking lots.

C. Pedestrian Connections *continued*

Guidelines

3. On-Site Pedestrian Circulation

Provide pedestrian paths or walkways connecting all businesses and the entries of multiple buildings on the same development site.

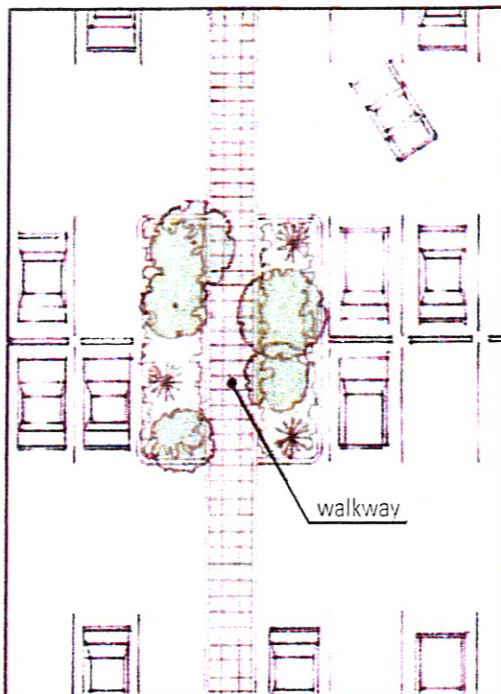
Provide pathways and crosswalks through parking lots along every fourth parking aisle or at intervals of less than 150 feet. Use contrasting surface materials for pathways to delineate pedestrian areas from travel lanes.

4. Site Integration with Transit Facilities

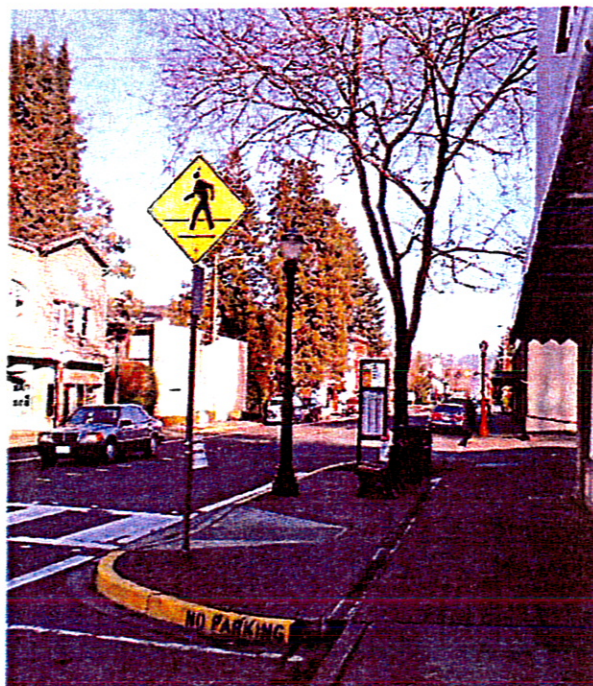
Integrate transit stops into the planning of adjacent site improvements.

- a. Extra space for waiting areas with benches, newspaper and bicycle racks, trash receptacles, a clock, bus schedule. Pavement for expanded waiting areas may be located within required setbacks and may count as landscape areas.
- b. Covered walkway directly from the transit stop into the project's entrance.
- c. Integration of transit shelter into the building by providing overhead weather protection (building canopy) with appropriate height and depth, and lean bars affixed to the base of the building.

Note: Reduction in required parking may be granted if convenient connections to transit are provided.



On-Site Pedestrian Circulation



Pedestrian connectivity near a transit stop

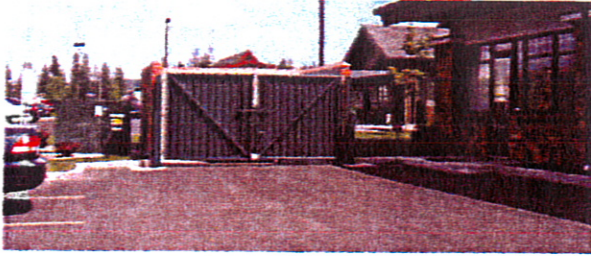
D. Screening of Trash and Service Areas

Intent

All unsightly uses (service, loading and trash collection areas) should be screened from the public realm in the Commercial area.

Guidelines

1. All service, loading and trash collection areas shall be screened by a combination of planting and architectural treatment. Examples include: masonry, wood, and fencing that complements the materials used in the development. Chain link fencing is prohibited.



Open and closed examples of trash screening areas

E. Maintain and Enhance Landscape Traditions

Intent

New development should build upon the rural character found within the local landscape to improve the commercial area's built environment and support the design intentions of a project's architectural character.

Guidelines

1. Landscape plans shall include existing vegetation on the site and ways to preserve significant and noteworthy plantings.
2. At a minimum, landscape plans shall include the following elements:
 - a. A unified pedestrian circulation system with amenities and plantings;
 - b. Plantings and/or site features that enhance the buildings' architectural qualities.



A pedestrian circulation system can integrate landscaping principles in a commercial setting

SHED DESIGN

BUILDING DESIGN

A. Massing

Intent

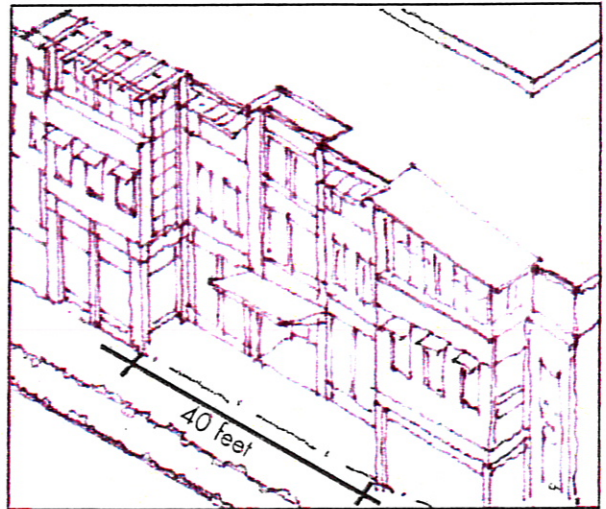
Give proper consideration to the scale and massing of individual buildings to be compatible with Black Diamond's desired architectural scale and character.

Guidelines

1. Design building volumes to maintain a compatible scale with their surroundings. In general, break down the scale and massing of larger buildings.
 - a. All new buildings with over a 100 ft. wall facing a public right-of-way shall modulate those facades visible from the public right-of-way every 40 feet.
2. Use articulated sub-volumes as a transition in size to adjacent historic or residential structures that are smaller in scale.
3. Express rooflines in a visually interesting manner that complements the composition of the building and the surrounding area:
 - a. Use a strong, detailed cornice or parapet in conjunction with a flat roof. Modulate the roofline at least every 100 feet.
 - b. Gable, hipped or shed roofs with a slope of at least 3 feet vertical to twelve feet horizontal are acceptable.
 - c. Other roof forms, such as arched, vaulted, dormer, or saw-toothed may be acceptable provided they don't exceed 100 feet without a change in plane or design.
 - d. Mechanical equipment shall be screened from the public right-of-way by an extended parapet wall or other roof form.



Buildings should match Black Diamond's small, rural character



Example of building facade modulation standard



Gabled roofline with differentiated slopes to articulate the building composition

B. Articulation

Intent

A set of responsive, regulating proportions will contribute to a coherent building design and promote cohesiveness within Commercial zones.

Guidelines

For buildings 25' or higher.

1. Emphasize the number of stories in a building using physical articulation, a change in materials, a belt course, or other methods that satisfies the intent.
2. Building facades abutting the street shall have an upper level setback at a height 25-40 feet above grade, with a minimum depth of 10 feet.



In this case, third story articulation breaks up the perceived bulk of the building

BUILDING DESIGN

C. Ground Level Details

Intent

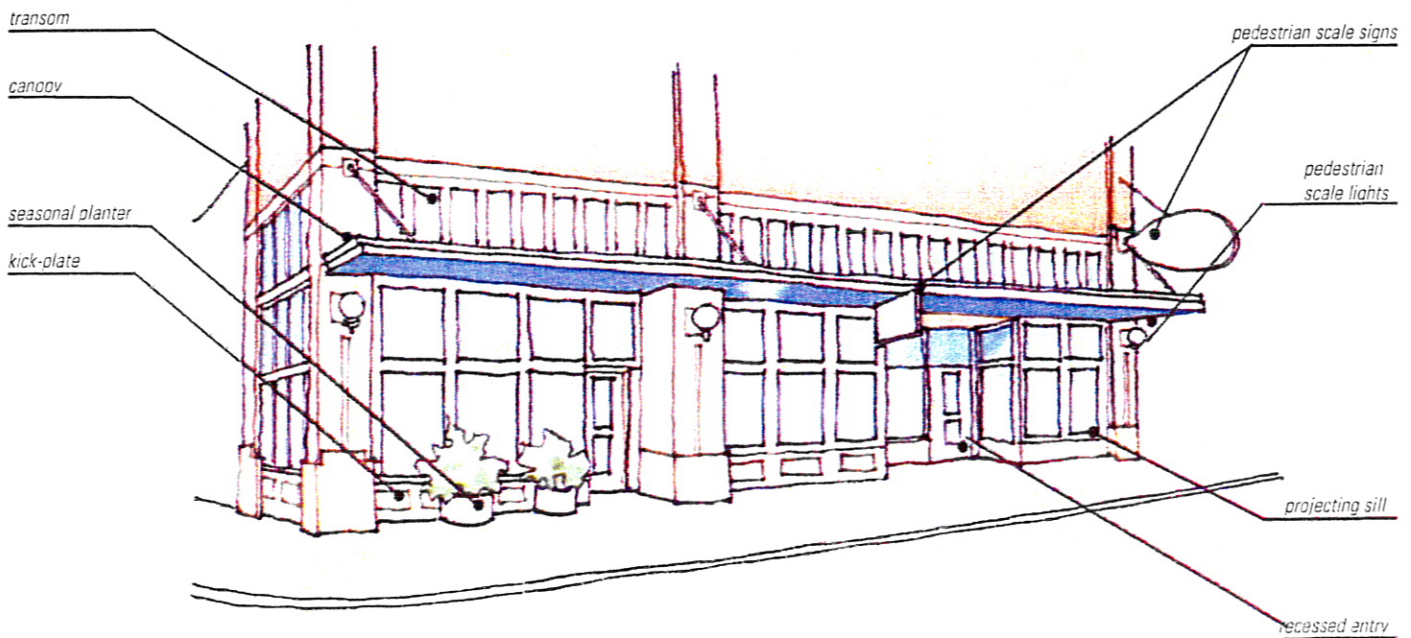
To create a socially and visually stimulating commercial district with street level facades that support pedestrian activity.

Guidelines

1. Promote active, pedestrian-oriented uses with a high degree of transparent window area. Reflective, opaque and highly tinted glass is prohibited.
2. Reinforce the character of the streetscape with the greatest amount of visual interest placed along the ground level of buildings. Facades of commercial and mixed-use buildings shall be designed to be pedestrian-friendly through the inclusion of at least three of the following elements:
 - a. kickplates for storefront windows
 - b. projecting window sills
 - c. pedestrian scale signs
 - d. canopies
 - e. transom windows
 - f. lighting or hanging baskets supported by ornamental brackets
 - g. an element not listed here that meets the intent



Good pedestrian oriented ground level detailing improves the street life of a commercial street



D. Blank Wall Treatments

Intent

To reduce the visual impact of blank walls by providing visual interest.

Guidelines

1. Blank walls longer than 30 feet facing streets or visible from residential areas shall incorporate two or more of the following:
 - a. Vegetation, such as trees, shrubs, ground cover and/or vines adjacent to the wall surface;
 - b. Artwork, such as bas-relief sculpture, murals, or trellis;
 - c. Seating area with special paving and seasonal plantings;
 - d. Architectural detailing, reveals, contrasting materials or other special visual interest.



Historic mural work can fit with the character of a commercial area and break up blank walls

BUILDING DESIGN

E. Corner Lots, Marking Gateways

Intent

To promote civic identity through prominent building features at important locations.

Guidelines

1. Buildings located at “gateways” or corners of blocks, should be given significant architectural expression in the facade, roof form, massing and orientation, such as:
 - a. tower forms
 - b. peaked roofs
 - c. larger entrances

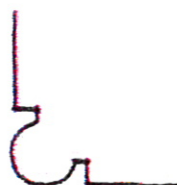
2. Gateways development shall also integrate special landscaping, artwork, and/or pedestrian open space along the street edge(s).



A significant corner building with unique architectural form can mark entry into the commercial area



curved



hinged



beveled



mitred



open bay w/ bevel

BLACK DIAMOND DESIGN GUIDELINES

for

Business Park/Industrial Areas

Business Park/Industrial Areas

Introduction and Purpose

This section of the Design Standards and Guidelines focuses on site planning and design guidance for the Business Park/Industrial Zones. Details on site design and building design are included in these guidelines to promote sensitive site planning and low impact development to preserve significant natural features and overall community character.

Site Design

Emphasis on the siting of buildings should be placed on clustering buildings and parking to preserve open space and significant natural features as community amenities, and to take advantage of opportunities to reflect and express the community's semi-rural character through the arrangement of buildings and landscape.

Building Design

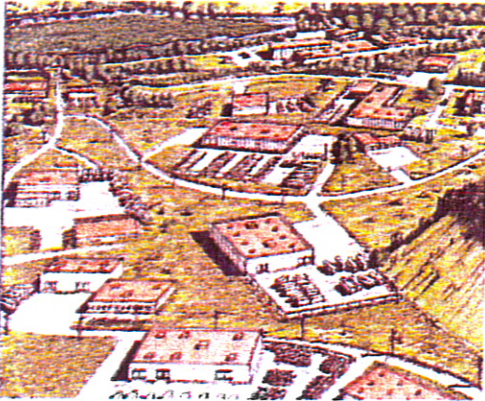
Typically, office campuses present a unified character to lend brand identity, professionalism and credibility. These guidelines stress a semi-rural, contextual approach to this development pattern with the following design directives:

- acknowledge and respect local natural features;
- design for compatibility with adjacent uses;
- design for overall cohesiveness;
- consider each building as a high-quality, long term addition to the city

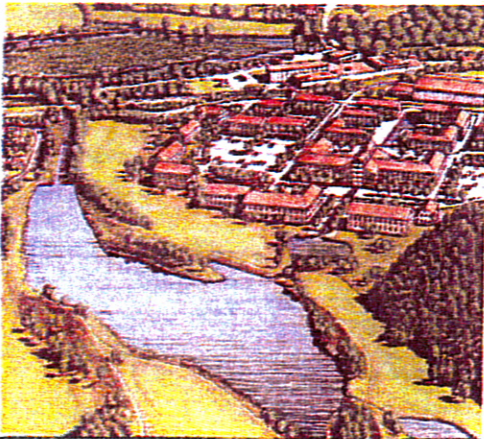
SITE DESIGN

Over-arching Site Design Intent

Emphasis on the siting of buildings within a business park should be placed on clustering buildings and parking to preserve open space and significant natural features as community amenities, and to take advantage of opportunities to reflect and express the community's semi-rural character through the arrangement of buildings and landscape. Site design should also recognize and relate to adjacent streets and adjacent developments.



Sprawling development patterns tend to ignore natural features and can negatively impact neighboring uses.



Integrated development can preserve and take advantage of unique natural features by clustering buildings, parking, access and circulation.



A. Integrated Site Planning

Intent

Each component of a business park and industrial campus should contribute to a coherent sense of the whole, including:

- encouraging cluster development to preserve open space and significant natural features as community assets;
- discouraging developments that result in “left-over” open space; and
- recognizing the important relationship between private, semi-public and public spaces in the arrangement of buildings, parking, service access, and pedestrian areas.

Guidelines

1. Preserve natural features (wooded areas, knolls, ponds and streams) and community landmarks (significant trees, farmhouses) by concentrating and clustering buildings on the land of least natural sensitivity/significance.
2. Establish clear pedestrian connections on site that are well-marked and ADA-compliant. Pedestrian paths or walkways should connect to all businesses and building entries, and through parking lots to direct pedestrians to buildings, streets and public spaces.
3. Discourage buildings that are internally focused at the expense of improving the pedestrian environment.
4. Buffer adjacent sensitive land uses from undesirable impacts that may originate from the site; buffers may be landscape and/or architectural in character.

SITE DESIGN

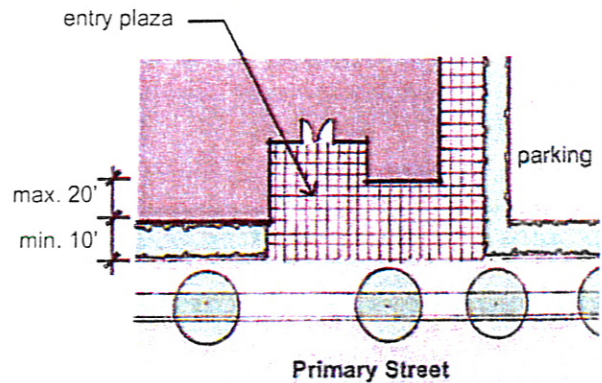
B. Building Orientation: Street Front

Intent

Buildings should be oriented with consideration to the visual impact from the perspective of the driver or pedestrian on the primary roadway. It is important to create the immediate first impression of a high quality business park, as part of the city's overall community character and identity.

Guidelines

1. Provide sidewalks along principal streets, including the following:
 - a. Sidewalk area with a clear zone of 5 feet for pedestrian travel;
 - b. A 4-foot-wide continuous planted area.
2. Require buildings to be developed with "principal" facades on the primary street. Establish a streetscape defined by attractive buildings located near the street edge within a landscape setting;
3. Site buildings should be sited in ways which make their entries or intended use clear to approaching visitors. Sole building entries from parking lots are discouraged.
4. Set buildings back at the corners of intersections from the corner property lines to allow for more generous sidewalk, additional street landscaping, and business signage.
5. Front internal access drives with a combination of buildings and landscaping and served by pedestrian walkways.



Building orientation and setbacks on primary street



Walkways, planting strips and a variety of trees make internal drives more visually appealing and safer for pedestrians

SITE DESIGN



Low masonry wall with landscaping

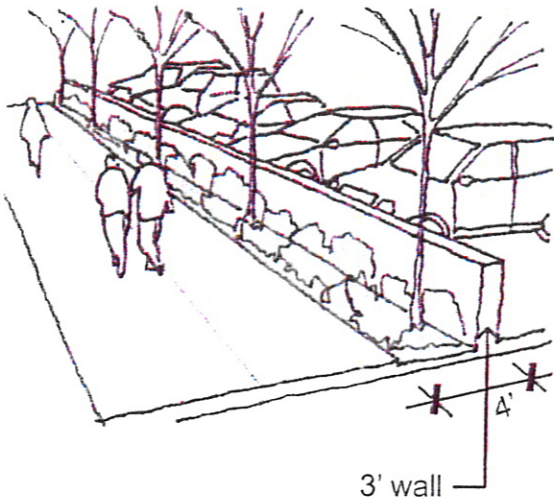
C. Parking Lot Screening

Intent

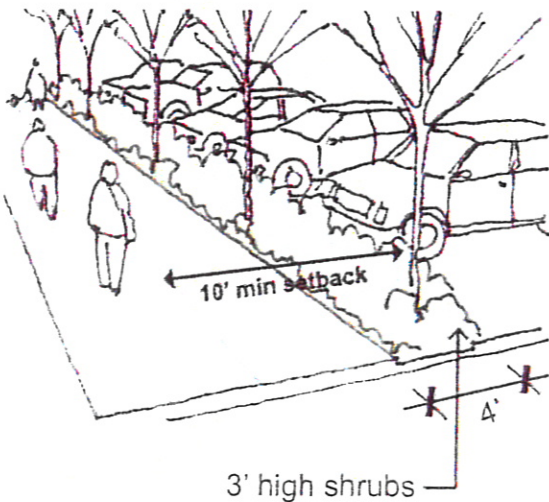
To reduce the impacts of surface parking while also maintaining visibility for surveillance.

Guidelines

1. Screen portions of parking lots that are in front of or beside buildings along the primary street(s) from the right-of-way with one or more of the following treatments:
 - a. Landscaping to sufficiently screen the first 3 feet in height adjacent to the parking area, within a 4' planting area.
 - b. Low walls made of concrete, masonry, or other similar material and not exceeding a height of 3 feet. Ground cover and trees must still be provided in the required 4' planting area.
 - c. Raised planter walls planted with a minimum 80% evergreen shrubs not to exceed 3 feet in height.



Surface parking from public view with wall and vegetation

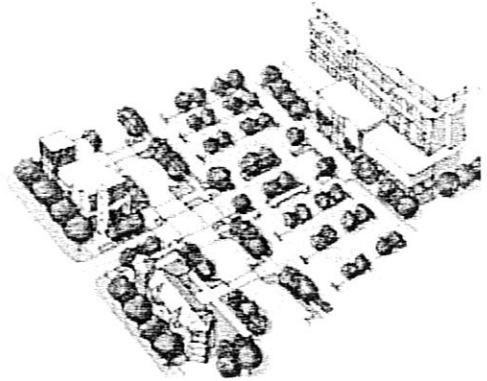


Surface parking from public view with vegetation

D. Parking Lot Location and Landscape

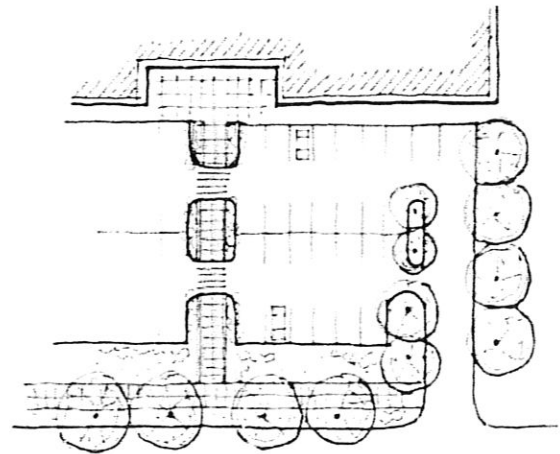
Intent

To reduce the visual impact of parking lots through landscaped areas that complement the overall design and character of development, providing both aesthetic and environmental benefit.



Guidelines

1. Locate parking lots to the side or behind buildings when possible. Parking located on the side or in front of the building shall meet the following:
 - a. Parking is set back a minimum of 10' from the property line;
 - b. Surface parking areas include 5' of perimeter landscaping and meet Guideline B when facing a Primary Street(s);
 - c. Parking area does not exceed 50% of the total frontage along the Primary Street(s); and
 - d. Promote shared parking access and shared parking among adjacent businesses.
2. Parking lots shall not abut street intersections (corner lots).



Screen parking from all neighboring properties and public rights-of-way with perimeter landscaping

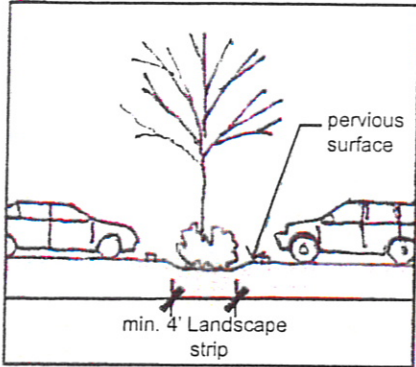
SITE DESIGN

D. Parking Lot Location and Landscape

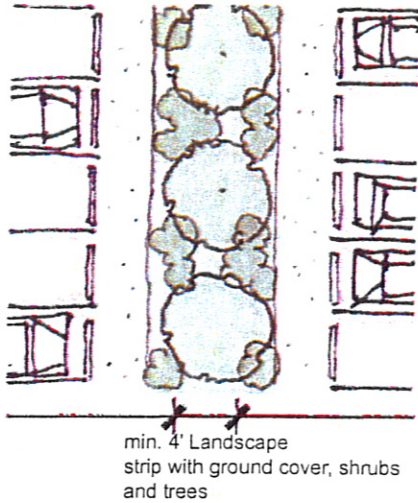
Guidelines

3. Dedicate 10% of all parking areas to interior landscaping, including the following:
 - a. Provide interior landscaping should be provided every 16 parking stalls and at the ends of each row of parking;
 - b. Landscaping strips should be a minimum of 4 feet wide and consist of ground cover, drought tolerant shrubs at a rate of 1 shrub per space, and at least one tree per every 6 spaces.

4. Allow the use of pervious pavers and other low impact methods of stormwater runoff infiltration in the design of parking areas to count towards the 10% interior landscaping requirement.



Interior parking lot landscaping strips



Interior parking lot landscaping strips, plan view



Landscaped 'fingers' break up the expanse of asphalt

E. Pedestrian Connections

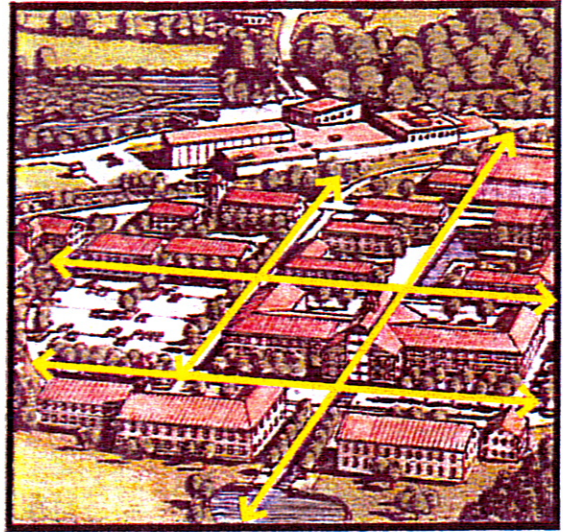
Intent

Business parks and campuses are enhanced when it is easier, safer, and more comfortable to walk between buildings and parking areas.

New developments should connect to surrounding uses, including streets, businesses, transit facilities, and residential areas, where appropriate.

Guidelines

1. Separate pathways through parking lots from vehicle parking and travel lanes by a combination of 2 or more of the following ways:
 - a 6 inch vertical curb in combination with a raised walkway.
 - a trellis, special railing, bollards, and/or other architectural features to accent the walkway at key points.
 - a continuous landscape area minimum 3 feet wide on at least one side of the walkway.
2. Use pathways to connect open spaces within the business park or campus, and link to adjacent trails, where appropriate. Use wayfinding elements, including signage and paving inlays to direct visitors to specific businesses and buildings within a park or campus.
3. Reinforce pedestrian connections with pedestrian-scale lighting for safety. Pedestrian scale lighting should be a maximum of 16 feet in height.



Pedestrian connections provide safe and comfortable access within a campus.



Connections can be naturalistic in character

SITE DESIGN

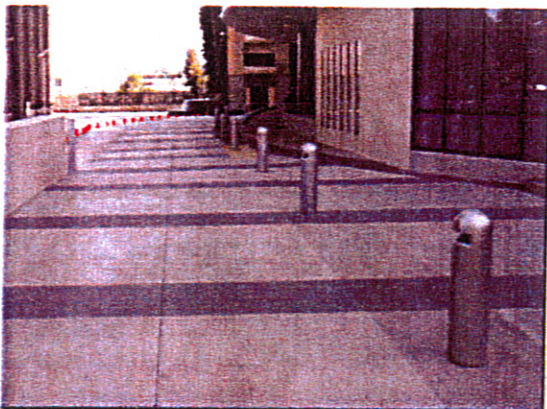
F. Site Lighting

Intent

Lighting is an important aspect of overall site development as it contributes to a feeling of safety and consistent “site vocabulary” to create a campus identity.

Guidelines

1. Short masted and local area lighting are the preferred lighting standards. Short masted lights should not exceed a maximum of 16 feet in height. Parking lot lighting should not exceed 30 feet in height.
2. Mount site lighting required for loading, staging and storage areas on the building facades if facing towards the site. The lighting should not be oriented towards any public right-of-way or adjacent properties.
3. Integrate building mounted lights should be integrated into the building character.
5. Use bollard lighting for pedestrian pathways and sidewalks.



Various lighting methods including bollard lighting for pedestrian sidewalks

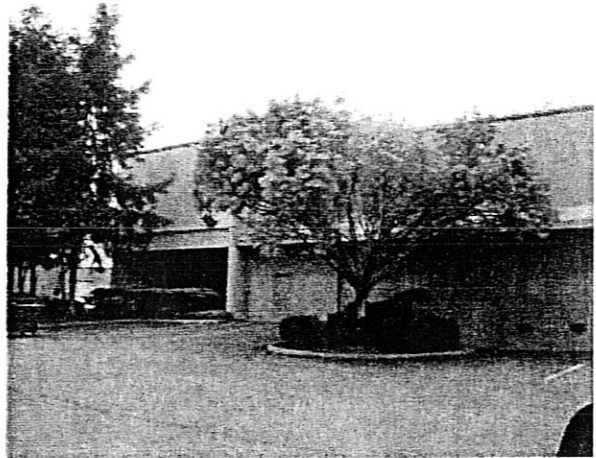
G. Screening of Trash, Loading and Service Areas

Intent

To reduce the impact of service, loading, and trash/recycling areas.

Guidelines

1. Screen service, loading and trash/recycling collection areas from public view with solid evergreen plant material or architectural treatment similar to the design of the adjacent building.
2. Loading and service areas should not face any residential district, unless no other location is possible. See Figure ___ for screening requirements if adjacent to residential zones.



H. Ancillary Uses and Storage Facilities

Intent

An important objective of the Business Park and Industrial design guidelines is to minimize clutter that can be associated with these uses.

Guidelines

1. Consolidate outdoor storage areas should be consolidated into a single area, and screened from the street and/or neighboring uses by buildings and/or landscaping.



Examples of landscaping screening methods

BUILDING DESIGN

Over-arching Building Design Intent

A clear visual and functional relationship between buildings, grounds, security and layout creates a cohesive campus. These guidelines emphasize a consistent "site vocabulary" to unify the site with similar signage and graphics, hierarchical paving, site furniture and landscaping. Within this concept, design emphasis is placed on building massing to complement the site furnishings, while distinctive design treatments in individual buildings can add three-dimensional quality and variety in character.



Example of simple modulation with deep openings that create shadow lines, provide visual relief that is carried to the top of the building.



Semi-rural character expressed in the form and roof line of a commercial building to help reduce apparent bulk

A. Massing and Bulk

Intent

The form and articulation of Business Park and Industrial buildings are expected to contribute to the desired campus environment. Large, monolithic buildings are discouraged.

Guidelines

1. Break down the scale and massing of larger buildings by modulating the building, both horizontally and vertically into smaller scale volumes. Carry this modulation up to the roofline.
2. Use floor to floor terracing on multi-story buildings to reduce bulk and increase amenity space as well as views.
3. Step buildings down towards surrounding residential uses and orient massing towards taller buildings nearby.
4. Design roofs to complement the composition and form of the building and the surrounding area. Gable or hipped roofs should be used if interpreted in a manner appropriate for an office or industrial use (such as metal materials). Use a strong, detailed cornice or parapet in conjunction with a flat roof. Employ high quality roofing material for roofs that are visible from the street.

BUILDING DESIGN

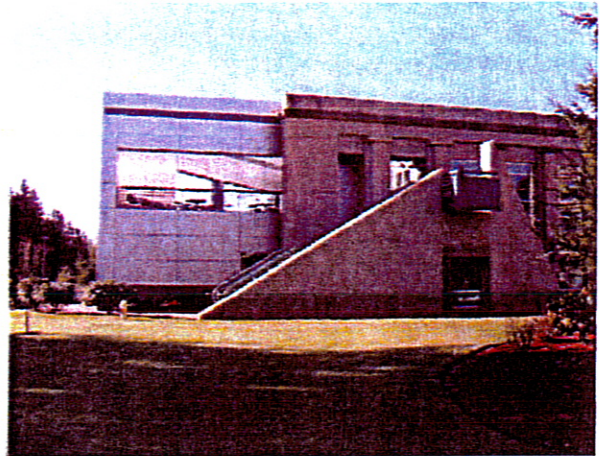
B. Form and Articulation

Intent

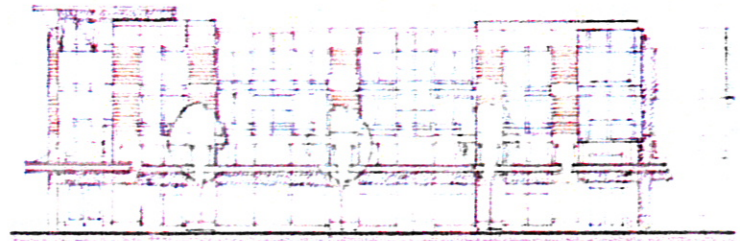
Reduce the apparent bulk of multi-story buildings and add richness and variety to Business Park/Industrial Campus architecture.

Guidelines

1. Maintain similar style, materials and scale with surrounding buildings. Use regulating lines such as soldier courses and rhythms like columns to break up long facades.
2. Provide a clear pattern of building openings. Windows, doors and other openings should unify a building's facade and add considerably to the facade's three-dimensional quality.



Unique architectural articulation reduces the perception of building bulk



Terracing provides visual relief and amenity

BUILDING DESIGN

C. Building Entrances

Intent

Provide well-marked, articulated entrances oriented to public spaces.

Guidelines

1. Orient main building entrances on either the primary street or main internal vehicle drive.
2. Accentuate the entrance(s) to a building's main lobby or interior office space; these main building entrances should be prominent in terms of size, articulation and use of materials.
3. Encourage the use of highly crafted materials or civic art pieces to further enhance the appearance and prominence of entrances.



Double height glazing, columns and landscaping give prominence and identity to a main building entrance



Separate entrance structures increase the prominence of building entrances

BUILDING DESIGN

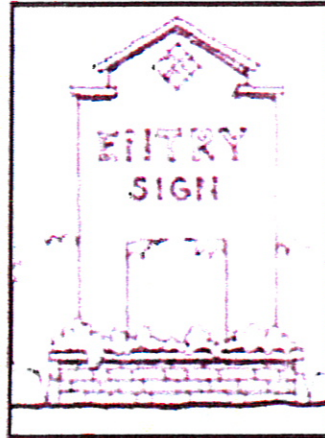
A. Sign Design

Intent

To ensure that business park and industrial campus signs are not solely oriented to automobile traffic.

Guidelines

1. Pole signs are prohibited. Ground signs should be no higher than 6 feet, and an integral part of the development's architectural design.
2. The base of any ground sign should be planted with shrubs and seasonal flowers.



Various sign styles that cater to pedestrians and motor vehicles

CITY OF BLACK DIAMOND

MASTER PLANNED DEVELOPMENT FRAMEWORK DESIGN STANDARDS & GUIDELINES

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INTRODUCTION

The Master Planned Development (MPD) Framework Design Standards and Guidelines are intended to provide guiding principles for the overall design of MPD applications within the City. These guidelines are to be followed in consideration of an MPD at both the initial and subsequent phases of approval. It is anticipated they will be supplemented by additional guidelines and standards that are developed when more specific plans for phased development are proposed. Those guidelines may be initially drafted by the MPD developer for consideration by the City prior to eventual adoption as part of a development agreement. As such, these guidelines are not intended to address all potential aspects of future development, but to provide an overall framework upon which additional guidelines may be added to in the future.

The more specific guidelines that are included at this time reflect important issues to the community which need to be carried forth in future amendments.

The statements contained herein are intended to be standards and guidelines, rather than prescriptive rules, and thereby provide an amount of flexibility. Any decision regarding strict application of any guideline contained herein will be made by the City Council as part of its consideration of granting overall MPD approval.

GENERAL SITE PLANNING

USING OPEN SPACE AS AN ORGANIZING ELEMENT

Black Diamond has a specific history and setting that involves varied topography, an agricultural past, forested areas, mining, and a small town scale. Care should be taken to reflect these patterns in master planned developments. In addition, the MPD chapter of Black Diamond's Municipal Code requires that fifty percent (50%) of the total land area of an MPD be maintained as open space. Proper design and integration of this open space into a development is very important.

INTENT

To protect sensitive environmental features and use open space to emphasize community activity and maintain a traditional and compact small town setting.

GUIDELINES

1. *All master planned developments shall include a wide range of open spaces, including the following:*

- *Sensitive environmental features and their buffers*
- *Green belts*
- *Village greens*
- *Parks and school playgrounds*
- *Public squares*
- *Multipurpose trails*
- *Wide planted medians in major streets*

These features should be deliberately planned to organize the pattern of development, to punctuate intersections and serve as center pieces to development clusters, not merely as "leftover" spaces.

2. *Open spaces shall be linked into an overall nonmotorized network through sidewalks, trails and parkways. This network shall be delineated at initial MPD approval and implemented through subsequent plats and permits approvals.*

3. *Stands of Trees as an Element of Open Space*

Due to the propensity of severe wind events in the Black Diamond area, an MPD should incorporate the preservation of larger rather than smaller stands of native trees.

INTEGRATING DEVELOPMENT WITH OPEN SPACE

INTENT

To allow for an efficient use of land, lower the cost of infrastructure and construction, protect environmentally sensitive areas, and maintain a small town “village” character within an MPD. Development is to be integrated with networks of preserved natural features and developed open space for both passive and active recreational uses.

GUIDELINES

1. *Use of conventional, suburban-style subdivision design that provides little common open space, shall be avoided. .*

2. *Groupings of primarily residential development of approximately 400-600 units should be contained generally within an area of a quarter mile radius to support walking and future transit service. Development clusters shall be surrounded by a network of open space with a variety of recreational uses including trails to provide connections between clusters.*

3. *Methodology for Planning Development in Clusters*
 - a. *environmentally sensitive areas to be protected (including streams, wetlands, steep slopes, wildlife corridors, and their buffers) shall be identified, mapped and used as an organizing element for design.*
 - b. *areas for development of housing and commercial development shall be indicated*
 - c. *streets and public spaces (as well as sites for public facilities such as schools, fire stations and other civic structures) shall be identified*
 - d. *lots and groups of lots with various ownerships (i.e. fee simple by occupant, condominium, single ownership apartments, etc) shall be integrated with one another throughout all phases of a project.*
 - e. *views of Mt. Rainier and other desirable territorial views shall be identified and integrated into site planning to maximum viewing from public spaces (streets, trails, parks, plazas, etc.).*

ENSURING CONNECTIVITY

INTENT

To promote ease of mobility and access within all portions of the development.

GUIDELINES

1. Pedestrian Connectivity

Similar to a traditional small town, services and common spaces shall be easily accessible to residents on foot. Off-street pedestrian trails are to be provided as a network throughout the development. Pedestrian connections shall be provided where cul-de-sacs or other dead-end streets are used.

2. Street Connectivity

The system of streets shall demonstrate a high degree of both vehicular and pedestrian connectivity, allowing residents and visitors multiple choices of movement. Isolated and dead-end pockets of development are not desired.

Cul-de-sacs shall be avoided unless there are no other alternatives .

MIXING TYPES OF HOUSING

INTENT

To encourage a diversity of population and households within Black Diamond through a range of choices in housing types and price.

GUIDELINES

1. MPDs shall include various types of housing, such as:

- Single Family, detached, on various-sized lots ranging from 2400 sf to 7200 sf.
- Single Family, attached:
 - duplexes*
 - townhouses (semi-attached)*
 - row houses (attached, common walls)*
 - courtyard houses*
- Cottage Housing
- Apartments
- Accessory Dwelling Units

2. Each cluster of development shall include a variety of unit types and densities.

3. *For single family developments, alley access to garages is desired. Direct driveway access to streets should only occur if there are no other alternatives.*

4. *Large apartments complexes and other repetitive housing types are discouraged. Apartments should replicate features found in single family residential areas (i.e., garages associated with individual units, individual outdoor entries, internal driveway systems that resemble standard streets, etc.).*

CREATING NEIGHBORHOOD CIVIC / COMMERCIAL CENTERS

INTENT

To conveniently concentrate services and activities to serve multiple residential clusters.

GUIDELINES

1. *Civic / Commercial Centers shall be located to serve groupings of clusters as well as pass-by traffic in order to support an array of shops and services.*

2. *Such centers shall be anchored by a public green space and, ideally, a public building such as a school or meeting hall.*

3. *Housing over retail or commercial spaces is strongly encouraged within Civic / Commercial Centers.*

CIRCULATION

STREETS

INTENT

To establish a safe, efficient and attractive street network that supports multiple choices of circulation, including walking, biking, transit, and vehicles.

GUIDELINES

1. Connectivity

The streets layout shall create a network that promotes traffic circulation and is well connected to other existing City streets

2. Design

- *The layout of streets should relate to a community-wide focal point.*
- *A consistent overall landscape theme should be utilized, with variations provided to indicate passage through areas of different use, densities, topography, etc.*
- *Prohibit the use of backyard fences or solid walls along arterial streets.*

3. Reduced Pavement Widths

Pavement widths should be minimized to slow vehicular speeds and maintain an area friendly to pedestrians and nonmotorized users.

4. Low-Impact Design

Stormwater runoff should be reduced through "natural" techniques: flush curbs, biofiltration swales, use of drought-tolerant vegetation, etc.

5. Traffic Calming Methods should include:

*Roundabouts
Traffic Circles
Chicanes
Corner bulbs*

6. Lanes and Alleys

Access to rear residential garages and commercial loading and service areas shall be available through lanes and alleys

7. Pedestrian Circulation

All streets shall include either sidewalks or trails on at least one side of the street.

8. Street Landscaping

All streets shall include native and/or drought-tolerant vegetation (trees, shrubs and groundcover) planted within a strip abutting the curb or edge of pavement. Native and/or drought-tolerant vegetation shall also be used within medians.

9. On-Street Parking

Curbside parallel parking shall be included along residential streets. Parallel or angle parking should be included within non-residential areas.

SIDEWALKS

INTENT

To provide safe, continuous pedestrian linkages within the street right-of-way.

GUIDELINES

1. Width

The minimum clear pathway shall generally be between 5' and 8'. Additional width between 4' and 20' is required for trees and planting strips within the right-of-way.

2. Lighting

All lighting shall be shielded from the sky and surrounding development and shall be of a consistent design throughout the development.

3. Furnishings

Street furnishings including seating, bike racks, and waste receptacles shall be located along main streets in Civic/Commercial areas.

Furnishings for businesses (outdoor seating) will require a building setback and cannot result in any obstruction of the sidewalk.

WALKWAYS AND TRAILS

INTENT

To provide safe, continuous pedestrian linkages throughout and sensitive to the project site, open to both the public and project residents.

GUIDELINES

1. Location

Walkways and trails shall be integrated with the overall open space network as well as provide access from individual properties. Trail routes shall lead to major community activity centers such as schools, large parks and shopping areas.

2. Width

Not less than 8' wide

3. Materials

Walkways connecting buildings and hardscaped common spaces shall have a paved surface

Trails throughout the development and connecting to larger landscaped common spaces shall be of at least a semi-permeable material

II. STANDARDS FOR PROJECTS

SITE DESIGN

CLUSTER DEVELOPMENT

INTENT

To ensure that development is compatible with the small town character currently found within Black Diamond.

GUIDELINES

1. *Larger groupings of development should be divided into smaller neighborhood clusters of 20-50 units defined by open space.*

2. Clustering

Within projects, higher density residential development shall be designed to have a village-like configuration. This includes elements such as:

- *Houses of varying sizes, styles, and form;*
- *The maximum number of attached units shall not be more than 8 within a single structure.*

NEIGHBORHOOD COMMON SPACE

INTENT

To provide a variety of usable and interesting open space(s) that supports an active community.

GUIDELINES

1. Amount

In general , within higher density residential and commercial development, a minimum of 1% of the lot area plus 1% of the building area shall be the amount of area set aside for common space

2. Location

Common open space shall be accessible and visible to users, as well as integrated into the overall project through connections and trails

3. Landscaping / Hardscaping

Commercial areas shall provide common space in the form of plazas, courtyards, and/or seating areas including some of the additional features noted below

Higher density residential areas shall have usable outdoor spaces that provide at least four of the following features to accommodate a variety of ages and activities:

Site furnishings (benches, tables)

Picnic areas

Patios or courtyards

Gardens

Open lawn with trees

Play fields

Special interest landscape

Public art

Water feature(s)

Sports courts such as tennis, basketball, or volleyball

4. Lighting

Pedestrian scale, bollard, or other accent lighting may be incorporated into the design of open space.

LANDSCAPING & PLANTING DESIGN

INTENT

To provide well-designed public parks and greens within the development.

GUIDELINES

- 1. Incorporate native, drought-tolerant vegetation; avoid extensive use of lawn and plantings that demand significant irrigation and fertilization.*
- 2. A minimum of 75% of the landscaped area (not including recreational areas) should be planted with plants other than turf or lawn. Perennials and/annuals are encouraged to provide special interest and highlight pedestrian areas such as walkways and trails.*
- 3. Where landscape areas are located adjacent to a street right-of-way, the type of landscaping should provide a vertical buffer.*

4. *Rocks, pebbles, sand, and similar non-living materials shall not be used as groundcover substitutes, but may be used as accent features provided such features do not exceed a maximum 5% of the total landscape area.*

STORMWATER DETENTION / RETENTION PONDS

INTENT

To integrate stormwater facilities as project amenities.

GUIDELINES

1. Location

Use natural site topography plus low-impact development methods to determine appropriate locations, which is to be integrated into the overall project design

2. Landscaping

Where possible, provide facilities that are site amenities, in order to reduce need for fencing. In general, public access to stormwater facilities should be included within design.

3. Fencing

Chain link fencing shall not be allowed. Other forms of non-obscuring fencing may be permitted when ponds exceed a safe slope. However, it is generally expected that ponds will be gently integrated into the design of the site with slopes that are safe to traverse on foot (less than 7% grade).

BUILDING DESIGN

ARCHITECTURAL FORM AND CONSTRUCTION

INTENT

To ensure that new development complements and strengthens the character of Black Diamond.

GUIDELINES

1. Variety of styles.

Provide a variety of building solutions through the mixing of one and two-story building profiles. Limit the amount of replication of building styles within one block.

2. Setbacks of Houses to Create a Sociable Environment

The front facades of houses should be setback between 5 and 15 feet from the back of the sidewalk. Vary front and side yard setbacks from house to house to provide interest and variety.

3. Setbacks of Garage to Reduce Visual Impact

The preferred location for garages is at the rear of the lot, with vehicular access being provided from an alley. Garage doors should be within 10 ft of the alley. If that is not possible, then garages shall be setback at least 20 feet to the front back of the sidewalk. That distance can be reduced when garage doors do not face the street.

4. Architectural Features

- *Housing shall include features such as:*
 - *Dormers*
 - *Brackets supporting roof overhangs*
 - *Corner boards*
 - *Wide trim around windows*
 - *Railings around balconies and porches*
 - *Low picket fencing*
- *Fronts of houses shall face the street and incorporate usable porches, stoops, and steps.*
- *Upper floors of houses shall be smaller than the floors below.*
- *Orientation of ridgelines of homes shall be varied.*

5. Materials

Wood frame construction with horizontal wood siding is the predominant building material used in Black Diamond, and shall be incorporated into new building design.

RESIDENTIAL BUILDING DESIGN

INTENT

To allow for maximum flexibility in location, size, and configuration of houses while ensuring that residential structures are in scale with lot sizes.

GUIDELINES

1. Floor Area Ratio (FAR) (Building Size to Lot Size)

FAR for detached residential development should not exceed 0.5;

Attached forms of residential may be up to 1.0 FAR;

Within Commercial/Civic Centers, residential development FAR may be as high as 2.5.

2. Height

Minimum 1 1/2 stories

Maximum 2 1/2 stories

3. Massing

Horizontal facades longer than 30' shall be articulated into smaller units, using methods such as: distinctive roof forms, changes in materials and/or patterns, color differentiation, and recesses or offsets.

4. Roof Pitch

May range from 6:12 to 12:12

5. Architectural Features

Front Porches - at least 6' in depth (or deep enough to allow for seating)

Garage Location the main house floor area shall extend at least 5' closer to the front lot line than any garage with street-facing doors. Design measures should be used for de-emphasizing garages, such as: porches, trellises, location of entry, break up massing/doors for double garages, overhanging second floor.

**NON-RESIDENTIAL BUILDINGS SHALL BE SUBJECT TO THE CITY OF
BLACK DIAMOND DESIGN GUIDELINES**

